

Carpinteria Rincon Trail



Artist's representation of proposed trail bridge



Project Study Report
Prepared by the City of Carpinteria
September 30, 2009

Carpinteria Rincon Trail

Project Study Report

City of Carpinteria
September 30, 2009





Carpinteria Rincon Trail

Table of Contents

Cover Page	1
Need and Purpose.....	4
Background and Project History.....	8
Permits and Environmental Studies	11
Plans, Specifications and Estimates.....	14
Acquisition of Right-of-Way.....	17
Construction Management and Engineering.....	24
System Planning.....	26
Environmental Resources, Issues and Potential Issues	29
Potential Hazardous Materials.....	31
Funding/ Scheduling	33
Attachments	36



Carpinteria Rincon Trail

Need and Purpose

The City of Carpinteria is renowned for its coastal setting with magnificent views of the Los Padres National Forest and the Santa Barbara Channel National Marine Sanctuary. Carpinteria's beaches are famous in their own right, being selected as among the nation's top 20. Carpinteria's downtown can be characterized as small with a variety of quaint locally owned businesses. Just down the coast from Carpinteria lies Rincon Point and



Rincon County Park. This coastal resource is world famous as one of the top surf spots in California.

Access between the City and Rincon County Park has traditionally been by freeway even though they are less than two miles apart. The use of the US 101 freeway, however requires the use of a car or the courage to ride your bicycle along the shoulder of the freeway. Many have found the railroad as an alternative route that's even more dangerous than the

freeway. Using the railroad corridor is perilous, yet the trail there affords great scenic beauty. Indeed, a well worn unsanctioned trail is present along the railroad connecting Carpinteria residential neighborhoods and Rincon County Park.

The proposed Rincon Hiking and Bike Trail provides the solution to eliminate public safety concerns as well as providing a strategic addition to Carpinteria's Coastal Vista Trail that one day will connect Padero Lane to the west and Rincon County Park to the east.

Public Safety. The most traveled way to hike or bike to Rincon County Park from the City of Carpinteria is along the railroad corridor, which is ill-advised and extremely risky. The railroad here is a major north / south rail route with freight and passenger trains running frequently each day. Recent projections in railroad use indicate the frequency of trains will dramatically increase by almost double the current usage by 2020¹. Unsanctioned use of the railroad corridor for beach access has recently been made even more dangerous since the Union Pacific Railroad installed continuous rail that is far quieter thus allowing trains to rapidly overcome unwary pedestrians. Despite all of these facts, it is common to see individuals and groups walking and biking along the tracks down to Rincon. Many trail users are carrying surfboards and day packs making them even more vulnerable to the speed of oncoming trains. Unfortunately, within the Carpinteria Community, four rail deaths have occurred in the last six years.



¹ Reference: Final Report, Coastal Access Study prepared by HDR, Inc. for the City of Carpinteria 2009.

Local Environment Enhancement. Bicycling and walking are cost effective, energy efficient, and clean and healthy ways to travel. Carpinteria's mild Mediterranean climate coupled with its scenic coastal bluffs provides an excellent environment for biking and walking. Bicycles also provide easy mobility for residents and tourists who don't drive or can't afford a car. This trail will provide a means for them to access Rincon without use of a polluting car.

Completion of this trail segment will provide critical improvements in public safety, provide greatly improved public coastal access to an area of spectacular natural beauty, reduce reliance on motor vehicles and improve public health through exercise and reduced air pollution.

The proposed trail, from the eastern end of Carpinteria Ave to Rincon County Park, will provide a hiking and biking connection between Ventura and Santa Barbara Counties. Near the western end of the trail is the Carpinteria Bluffs Nature Preserve which provides visitors with a unique overlook along one of the last remaining undeveloped coastal regions along the South Coast. Commonly seen from the bluffs are osprey, turkey vultures, red-tailed hawks, American kestrels, brush bunnies, coastal bottlenose dolphins, California sea lions, Pacific harbor seals, California brown pelicans, western gulls and migrating gray whales. This trail provides an extension to Carpinteria Bluffs trail system, allowing hikers and bikers to continue on their journey, without the use of a car, down along the coastal bluffs into Rincon Park. The trail will feature environment attributes that include; a storm water pollution remediating bioswale, native plant restorations, a rain water collection cistern to provide summer irrigation water and some of the most spectacular coastal scenery anywhere.

Recreational Purposes. During the summer, Carpinteria is an attractive getaway, with close to two million visitors a year. Its City beach is recognized as one of the safest and cleanest beaches in the country and was



recognized as the cleanest in Santa Barbara County. Northeast of the City beach is the Carpinteria Salt Marsh Nature Park, one of the healthiest of the few remaining coastal wetlands in California and a popular hiking area. East of the City beach is the Carpinteria State Beach, an increasingly popular camping and recreation area. Within the state beach, the Carpinteria Creek flows out to the ocean. This creek is the focus of a steelhead restoration effort.

Steelheads are a federally listed endangered species. East of the state beach, the coastal

bluffs begin, where the Casitas Pier is located. Part of the coastal bluff is also the Carpinteria Bluffs Nature Preserve. Located just east of the Casitas Pier is the Pacific harbor seal sanctuary, a natural haul out and pupping rookery where over 500 of these

pinnipeds have been observed on shore. Offshore, the Santa Barbara Channel National Marine Sanctuary and Channel Islands National Park provide extraordinary opportunities; indeed, it is the natural beauty that defines this magnificent place. The City of Carpinteria aims to complete the Coastal Vista Trail for the recreational use of the surrounding communities and so that these natural places of interest are accessible to everyone.

The proposed trail would begin at the eastern end of Carpinteria Avenue near the intersection of highway 150 and US. 101. The trail would then head east toward Rincon Point, turning around a knoll. From there it would descend to connect to where a proposed pedestrian bridge would cross above the Union Pacific Railroad tracks. The hiking and biking trail would then meander through coastal sage scrub before reaching the popular County Park, picnic area, beautiful beach and world famous surfing destination.



Completion of this trail segment will provide critical improvements in public safety, provide greatly improved public coastal access to an area of spectacular natural beauty, reduce reliance on motor vehicles and improve public health through exercise and reduced air pollution.



Carpinteria Rincon Trail

Background and Project History

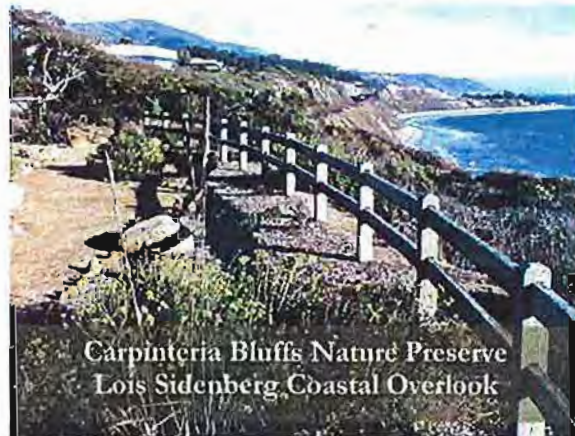
There is no acceptable option for those that wish to bike or walk to Rincon from Carpinteria. They are forced to use either the freeway shoulder or use the unsanctioned circuitous inland route on narrow roads. Given these two much less favorable options, many instead choose to drive to this beach where parking is often at capacity. Use of the proposed trail should reduce the impact on parking facilities at Rincon and help reduce vehicle emissions in the coastal zone. The proposed trail could also improve the access to scenic and remote coastal areas that are not served by roads, without impacts of vehicular use.

The nature of the property that will be included in the proposed project has changed greatly over the years; from the 1920's causeway, to the construction of Highway One and Highway 101, and the construction of the Pacific Coast Railroad. Despite the alteration of the adjacent topography, Rincon remains one of the most treasured surfing spots in the world and possesses unmatched scenic beauty of the Channel Islands National Marine Sanctuary.

State law provides, under the California Recreational Trails Act, for a coastal trail extending from the Mexican to the Oregon border, known as the Pacific Trail or the California Coastal Trail. It is estimated that this trail is about 80 percent complete. The City of Carpinteria has been working diligently on its own project, the Carpinteria Coastal Vista Trail (CCVT,) that provides a critical link in the State's Pacific Trail.

The CCVT is an area wide trail that has been in the works for over ten years and has already had approximately two miles completed. Most recently, a segment was completed in 2007. The ambitious yet obtainable goal is to provide a non-motorized trail route from the Carpinteria Salt Marsh Nature Park on the west end of Carpinteria to Rincon County Park. The trail route has many attributes. At the beginning is the Carpinteria Salt Marsh

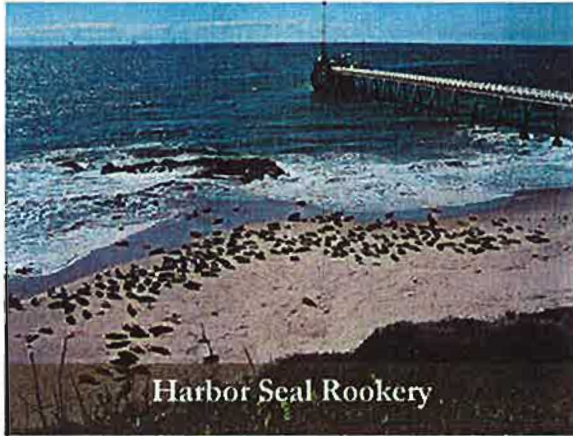
Nature Park, adjacent to the approximately 200 acre protected salt marsh, which features seven acres of restored wetlands including a tidal basin, flooding with each high tide. The marsh is home to several endangered species such as the Pigmy Blue Butterfly and the Savannah Belding Sparrow.



Moving eastward is the Palm to Linden trail, a project that the City is currently implementing; it was recently approved and has received funding, and will provide an internal link of the CCVT trail, from the Carpinteria downtown area to the State beach entrance. This project will feature a functioning bioswale that will include native plantings with local cultural significance such as Yerba Mansa (*Anemopsis californica*) and Basket Rush (*Juncus textillis*). All of this will be featured on interpretive educational signage. The Palm to Linden Tail will also feature an interpretive play area that is

closely themed to the local Chumash legend of the Rainbow Bridge. The Palm to Linden Trail will end where the CCVT trail continues through the Carpinteria State Beach with natural tar seeps and famous reefs and tide pools. From there the CCVT route passes through Tar Pits Park, an eight acre natural open space overlooking restored coastal sage scrub habitat.

Then the trail passes by the Carpinteria Harbor Seal Rookery that has seen as many as 500 adult and juvenile harbor seals on the beach during pupping season during the spring and fall molting season. The trail then connects to the 52 acre Carpinteria Bluffs Nature Preserve and the Sidenberg Coastal Overlook. This overlook provides panoramic ocean and island views where a hiker can see migrating grey whales with the back drop of the Santa Barbara National Marine Sanctuary. The proposed project, the Rincon trail, will be the final crucial link to popular beaches and one of the world's most famous surfing destinations. The proposed project will feature a picnic area, functioning bioswale,



Harbor Seal Rookery

storm water runoff management, bridge for safe railroad crossings and a minimal slope in all areas for maximum accessibility for hikers and bikers of all levels.

In the proposed project, the City has carefully developed plans to meet a critical need. The proposed project will help to improve pedestrian safety by eliminating incentives to use the railway corridor. The trail will reduce the use of motor vehicles in Carpinteria's coastal areas and improve the environment through improved water quality and reduced air pollution. As the City of Carpinteria continues to succeed in providing coastal access and coastal tourism, this trail will be cared for into the foreseeable future.



Carpinteria Rincon Trail

Permits and Environmental Studies

Completion of permits and environmental studies. The City of Carpinteria, Parks and Recreation Department has a history of park and trail development now consisting of over 100 acres of parks, playgrounds, and areas of open space.

The proposed trail will serve as a great asset to the Santa Barbara region and the community of Carpinteria. The City will comply with applicable environmental protection laws to ensure that the construction and operation of the trail does not have a significant negative impact on nearby environmental resources. The initial biological assessment, by VJS Biological Consulting, found that the trail route will not affect any sensitive species. The trail route does not cross any drainages or riparian corridors, and does not impact any wetlands. (See attachment #1). The proposed trail will be conveniently located on an old terrace cut for the previous railroad route; therefore in most areas there will be minimal land disturbance. At either end of the trail are pre-existing car parks; on the Rincon end is a paved lot and on the Carpinteria Avenue end is a dirt lot, which the City plans on improving with decomposed granite. Most of the vegetation along the side of the trail is common and will be able to be restored; native plantings are part of the proposed plans. The study found no sensitive species, although the CA legless lizard (*Anniella pulchra*) has been known to exist in the surrounding area. It is doubtful that any exist in the location of the current trail, due to the existing environment, which is unfavorable to the CA legless lizard. VJS speculated that some preconstruction raking could be done in order to confirm that these unusual lizards do not exist in the proposed trail corridor. In the unlikely event that the legless lizard was found in the area, disturbances could be mitigated by capture and relocation. It was also noted that very few trees exist along the proposed trail corridor that could be utilized for raptor nesting.

In a study conducted by Dudek in a Phase 1 Cultural Resources Study, the soil in the area of the proposed trail was characterized as Xerorthents, cut and fill areas consisting of mechanically manipulated soils where the original profile is no longer discernible. (See attachment #2). The study found that no prehistoric artifacts are mixed with modern debris and that the area has already been subject to extensive ground disturbances associated with construction. They concluded that no potentially significant cultural resources were identified and therefore the proposed project would have no significant impact on cultural resources.

The proposed plan will be subject to an initial study under CEQA to find what level of environmental review will be required. The City anticipates that the proposed plan's initial study will indicate a likelihood of no significant impacts. The City expects to prepare a negative declaration. In the event that the initial study does find that there might be some environmental impacts the City plans to utilize appropriate methods to mitigate any impacts and prepare mitigated negative declaration. The City of Carpinteria is committed maintaining healthy environments and ecosystems. City projects have implemented a variety of eco-services such as; bioswales, non-motorized trails, native plantings, and storm water runoff receptacles.

The proposed Rincon Trail is consistent with Section 30500(a) of the 1976 California Coastal Act which states that each local jurisdiction should prepare “*a specific public access component to assure that maximum public access to the coastal and public recreation areas is provided*”. The proposed trail will provide greatly improved coastal access and it will enhance coastal recreational opportunities in this region, consistent with coastal access provisions of the California Coastal Act. The City of Carpinteria anticipates that the county will grant a Coastal Development Permit for areas of the proposed trail within the County of Santa Barbara. The City of Carpinteria Parks and Recreation department anticipates that the City of Carpinteria will grant a Coastal Development Permit for the areas of the proposed trail within City limits.

The Regional Water Control Board requires that any projects that disturb one or more acres of soil obtain a General Permit; the proposed Rincon Trail would be subject to this. In order to obtain a General Permit it is required that a Storm Water Pollution Prevention Plan (SWPPP) be developed and implemented. The City has already taken this into account in the plans of their project by implementing a storm water cistern and a bioswale. The functioning bioswale will allow storm water to drain into it and be naturally filtered before being drained into the nearby creek and ocean. This will be particularly beneficial during the first seasonal flush, due to the fact that this storm water contains the most non-point source contaminant that includes bacteria, hydrocarbons, pesticides and nutrients. The bioswale will reduce contaminants exiting the site and improve water quality in the near by popular ocean areas. The storm water cistern will sit below the top ridge of the coastal bluff and capture runoff from the ridge top, it will hold approximately 10,000 gallons and will be stored to use for irrigation during dryer months. The project has been designed to utilize many best management practices (BMPs) which are also mandated under the SWPPP, from use of decomposed granite trail and the implementation of the storm water cistern. Consideration has been and will continue to be given to extending the use of BMPs to enhance the project.

Alternatives. Only one Alternative has been considered for this project, no-build. With a no-build alternative, bikers and hikers would have to continue to use the hazardous bike lane next to the free way. Hikers would have no option other than to drive or use the un-sanctioned trail currently existing, in which unlawful railroad crossings are being used, which is extremely risky. There would be no costs for this alternative, but no benefits as well.



Carpinteria Rincon Trail

Plans, Specifications and Estimates

Preparation of plans and specifications. In July of 2007, the city requested a preliminary landscape plan from Van Atta Associates, a landscape architecture and planning company. The Dudek preliminary plans (See attachment #3) include enhancement of a decomposed granite car-park that will lead to a 12' wide decomposed granite or permeable paving trail. The width will be wide enough for bikers and hikers to easily ride and walk side-by-side and pass others headed in the opposite direction. The trail width will also allow emergency vehicles to access the area. The maximum slope of the trail will be approximately 7% to 10% for maximum accessibility. A bridge will be used to safely cross users over the railway. Additionally, the plans feature a bioswale with native plantings and a 10,000 gallon storm water cistern used for irrigation.

**Preliminary Cost Estimate
for the City of Carpinteria's
Proposed Rincon Beach Trail
11/24/08**

Professional Costs

Final design (including Landscape)	\$ 100,000.00
Caltrans Permitting	\$ 20,000.00
Railroad Permitting	\$ 20,000.00
Easement/Right of way Agreements	\$ 15,000.00
Coastal Permitting	\$ 20,000.00
Bidding Documents	\$ 20,000.00
Subtotal	<u>\$ 195,000.00</u>
Construction Survey, CM, and Inspection	<u>\$ 200,000.00</u>
Total Professional Costs =	\$ 395,000.00

Construction Costs

Signage	\$ 4,500.00
Fencing	\$ 25,000.00
Drainage	\$ 35,000.00
Grading	\$ 250,000.00
Surface of Path (D.G.)	\$ 200,000.00
Striping	\$ 10,000.00
Mobilization	\$ 200,000.00
SWPPP	\$ 10,000.00
Bridge w/Abutments	\$ 70,000.00
Landscaping & Irrigation	\$ 50,000.00
AC Parking Lot	\$ 50,000.00
Retaining Walls	\$ 44,000.00
Lighting	<u>\$ 200,000.00</u>
Total Construction Costs =	\$ 1,148,500.00

Subtotal of All Costs	<u>\$ 1,543,500.00</u>
15% Contingency	<u>\$ 231,525.00</u>

Total Estimated Preliminary Cost = \$ 1,800,000.00



Carpinteria Rincon Trail

Acquisition of Right-of-Way

Acquisition of right-of-way. The proposed trail route crosses over several parcels of land already owned by public agencies. The trail starts on Carpinteria Avenue owned by the City of Carpinteria. It then courses down a hill that is owned by the State of California as part of the U.S. 101 ROW. From there the proposed route involves two parcels of land owned by Union Pacific Railroad (APN 001-010-032) and (APN 001-220-048) before the trail reconnects to a parcel of land owned by the County of Santa Barbara as part of Rincon County Park. In this area the trail would utilize a topographic bench that was abandoned by Union Pacific Railroad back in the early 1970's when the railroad was realigned as part of a freeway project.

The City of Carpinteria anticipates that Union Pacific Railroad would cooperate in the construction of the Rincon Trail because the proposed project would help to eliminate an un-sanctioned and hazardous current use of the railroad tracks as a means of pedestrians to access Rincon Beach. Construction of the overhead bridge would not interfere with railroad use.

The City of Carpinteria also believes the County of Santa Barbara Parks Department would partner in order to construct the proposed trail as it is consistent with Santa Barbara County Local Coastal Program goals and policies.

Please see photos of parcels of land involved in the Rincon Trail project.

- | | | | |
|-------------------------------------|-------------|--------------|--------|
| 1. Union Pacific Railroad | 001-220-092 | 2.819 Acres | pg. 19 |
| 2. Union Pacific Railroad | 001-010-032 | 8.3299 Acres | pg. 20 |
| 3. County of Santa Barbara | 001-220-048 | 11.2 Acres | pg. 21 |
| 4. City of Carpinteria Right-of-Way | | n/a | pg. 22 |
| 5. Caltrans Right-of-Way | | n/a | pg. 23 |



#1



2

©1997-2007 AIRPHOTO USA

#3



©1997-2007 AIRPHOTOUSA

4



#5



©1997-2007 AIRPHOTOUSA



Carpinteria Rincon Trail

Construction Management and
Engineering

Construction and construction management and engineering, including surveys and inspections. The City of Carpinteria is committed to the completion of the Rincon Trail and plans to implement responsible and efficient construction management and engineering. Initial surveys and inspections have been completed and once further funding is available additional surveys and inspections will be conducted.



Carpinteria Rincon Trail

System Planning
Coordination and Consistency with Statewide,
Regional, and Local Planning

The project is consistent with the City of Carpinteria General Plan, the California Coastal Act and County Local Coastal Program.

City of Carpinteria General Plan

Community Design Element Objective CD-8: *To encourage and facilitate pedestrian and bicycle pathways.*

Open Space, Recreation & Conservation Element Objective OSC-15: *Maintain the existing trail system and provide additional recreation and access opportunities by expanding the trail system.*

OSC-15-Implementation Policy 73: *As part of the development of the trail system, minimize the number of formal rail crossings for pedestrians and improve their safety through crossing controls or other improvements such as fencing and landscaping.*

OSC-13: *Preserve Carpinteria's visual resources*

C-2: *Designate scenic routes as to provide for the scenic enjoyment and maintain natural beauty of lands and views along roadways of the Carpinteria Valley.*

Santa Barbara County Coastal Plan

The proposed trail is consistent with Santa Barbara County Local Coastal Programs (LCP) goals and policies as demonstrated:

Goal 1.2(b): *Assure orderly, balanced utilization and conservation of coastal zone resources taking into account the social and economic needs of the people and the state.*

Goal 1.2(c): *Maximize public access to and along the coast and maximize public recreational opportunities in the coastal zone consistent with sound resources conservation principles....*

Policy 3.7-6: Coastal Access and Recreation
Recreational uses on oceanfront lands, both public and private, that do not require extensive alternation of the natural environment (i.e. tent campgrounds) shall have priority over uses requiring substantial alternation (i.e. recreational vehicle campgrounds).

Policy 3.7-8: Coastal Access and Recreation: Carpinteria Valley Planning Area
Increased opportunities for beach access shall be provided in the Carpinteria Valley Planning Area

California Coastal Act:

The project is consistent with Section 30500(a) of the 1976 California Coastal which states that each local jurisdiction should prepare “*a specific public access component to assure that maximum public access to the coastal and public recreation areas is provided*”.

The project is also consistent with the state wide California Coastal Trail. The California Coastal Trail (CCT) is one of the great trails of our nation. Once completed, it will extend 1,200 miles from Oregon to Mexico. While informal trails along our coast have been used for centuries, the CCT’s more recent history began in 1972 when Californians passed Proposition 20 recommending that trails system be established along or near the coast. In 1999, the CCT was designated at the state and federal level as the Millennium Legacy Trail, and in 2001 state legislation called for its completion. Today roughly half of the CCT is complete. The proposed Rincon Trail will be an important link in this grand plan.



Carpinteria Rincon Trail

Environmental Resources,
Environmental Issues and Potential
Environmental Issues

Inventory of environmental resources, identification of potential environmental issues and anticipated environmental processing type. Potential mitigation requirements and associated costs

Most of the proposed trail route is located along old roadways or old terraced road cuts. A small unsanctioned trail exists in some areas of the proposed trail. Most of the area has been mechanically manipulated over the years.

In the initial biological assessment, VJS Biological Consulting found that the great majority of vegetation in the area of the proposed trail is common vegetation for a coastal bluff area. (Attachment #3). Vegetation common to the area includes: Brewer's saltbush, coastal sage scrub, ice plant and eucalyptus trees. Some non-native species also exist in the area including Myoporum, a non-native shrub/tree, which was noted in increasing numbers in the area.

In the initial biological assessment, it was observed that common animal species exist in the area including western fence lizards, side-blotched lizards, CA ground squirrels, brush rabbits, song sparrows, black phoebe, turkey vultures, red-tailed hawks, mourning doves, house sparrows, CA towhee, and rough-winged swallows. They noted that a few species, such as wood rats, grey fox, raccoons, opossum, and probably gopher snakes and king snakes, utilize the coastal bluff area. VJS concluded that no sensitive species were observed during their visits. The legless lizard has been known to exist in the surrounding area, but it is doubtful that any exist in the location of the current trail, due to the existing environment, which is unfavorable to the lizard. The City of Carpinteria plans to rake preconstruction to confirm that these unusual lizards do not exist in the areas, which would be of minimal expense to the City. In the unlikely event that the legless lizard was found in the area, disturbances could be mitigated by capture and relocation, this would be of greater expense to the City, but is considered to be an unlikely situation.



Carpinteria Rincon Trail

Potential Hazardous Materials

Potential hazardous materials / waste problems and potential mitigation or avoidance and potential costs.

The City of Carpinteria does not expect to encounter any hazardous material waste in the construction of the proposed trail. The area has no history of dumping.



Carpinteria Rincon Trail

Funding/ Scheduling

The proposed Rincon Trail would benefit not only to the community of Carpinteria, also to those who visit Rincon from all over the world, biker enthusiasts and coastal hikers. Therefore many agencies, public and private, that would have an interest in the completion of this project.

The Santa Barbara County Association of Governments (SBCAG) is a regional planning agency comprised of Santa Barbara County and all eight incorporated cities within the county. SBCAG distributes local, state, and federal transportation funds and acts as a forum for addressing regional and multi-jurisdictional issues. In particular SBCAG is responsible for allocating funds from the Transportation Enhancement Activities (TEA) funds which is specific for bicycle, pedestrian, landscaping, public art or historic projects linked to transportation. The City anticipates that the proposed project would be a match for these funds.

The California Coastal Conservancy (CCC) awards grants to public agencies to achieve goals consistent with their mission: to preserve, protect and restore the resources of the California Coast. Their vision is of a beautiful, restored and accessible coastline. In particular, one of their goals is to achieve public access. Their goal is to achieve this by developing the Pacific Coastal Trail as a major new recreational amenity, tourist attraction, and alternative transportation system, especially in urban areas, and develop networks of inland trails that connect to the coast and parks and provide other recreational opportunities. The proposed trail would help to achieve this goal. The CCC aims to restore and enhance biological diversity in coastal watersheds; the City plans to help achieve this goal by native plantings and by implementing a functioning bioswale. Some examples of the kinds of projects the Coastal Conservancy fund include trails and other public access to and along the coast, natural resource protection and restoration in the coastal zone or affecting coastal areas, similar to the proposed project.

Measure A is a transportation measure which will provide more than \$1 billion of estimated local sales tax revenues for transportation projects in Santa Barbara County over 30 years. The Measure A Investment Plan will also provide \$455 million each for the North County and South Coast for high priority transportation projects and programs to address the current and future needs of local communities. In both regions, the plan provides funding for safer walking and bike routes. The proposed Rincon Trail would serve these purposes and could potentially receive Measure A funding.

Private Development Funds. The current owners of Bluffs Property III, located towards the eastern end of Carpinteria Avenue, are expected to submit a conceptual design for a resort to city planners, according to Sharon McCray who represents the owner of the 26-acre property. The proposed trail would be the only way of beach access for resort goers and the owners would have a vested interest in ensuring that the proposed Rincon Trail be completed.

Parks improvement funds. The City of Carpinteria has aggressively pursued parkland and habitat preservation projects over the last twelve years. The City has acquired and improved over sixty acres of open and park space in the last 15 years. The eight-acre Salt Marsh Nature Park, the eight acre Tar Pits Park and the 53-acre Carpinteria Bluffs Nature

Preserve are impressive examples of the City’s ability to protect and preserve precious coastal resources. The City is committed to the completion of this project and anticipates contribution of appropriate park funding.

Rails-to-Trails Conservancy is a nonprofit organization based in Washington, D.C., whose mission is to create a nationwide network of trails from former rail lines and connecting corridors to build healthier places for healthier people. They serve as the national voice for more than 100,000 members and supporters, 15,000 miles of rail-trail throughout the country, and thousands of miles of potential rail-trails to be built. The proposed trail is along the current rail corridor and will be located, in part, on areas that were at one point railroad tracks.

Federal Transportation Administration (FTA) helps communities support public transportation is by issuing grants to eligible recipients for planning, vehicle purchases, facility construction, operations, and other purposes. FTA administers this financial assistance according to authorization, which was signed into law in August 2005. Generally, FTA funds are available to designated recipients that must be public bodies (i.e. states, cities, towns, regional governments, transit authorities, etc.) with the legal authority to receive and dispense federal funds. Due to the nature of the proposed project alleviating parking and transportation needs within the county and the city, funds from the FTA could be possible.

The tentative milestone schedule is listed below. Target dates are dependent on funding.

Milestone	Target Date
CEQA Initial Study	June 2011
Environmental Clearance	September 2011
Initial Plans	January 2012
Final Plan Completion and Submittal	September 2013
Construction commence	January 2013
Construction complete	January 2014



Carpinteria Rincon Trail

Attachments

Attachment 1	Initial Biological Assessment
Attachment 2	Cultural Resources Survey
Attachment 3	Preliminary Plans
Attachment 4	Right-of-Way
Attachment 5	CD of studies & plans
Attachment 6	Current Pictures - Trail Route



Carpinteria Rincon Trail

Attachment 1 Initial Biological Assessment



November 17, 2008

Initial Biological Assessment Rincon Trail

For: Matt Roberts, Director of Parks and Recreation
City of Carpinteria

By: Vince Semonsen
Wildlife Biologist

Introduction: This letter report documents the biological resources observed along the proposed Rincon Trail. The trail will connect the eastern end of Carpinteria Avenue to the parking lot at Rincon. A site visit was conducted on July 16, 2008 with Matt Roberts, Director of Parks and Recreation for the City of Carpinteria. Any wildlife observed at, or expected to utilize the parcel will be addressed in this report, with attention given to the presence of any sensitive species. A general description of the vegetative habitat types will also be documented in this report.

The trail will be 10 – 12 feet wide and between 4,800 to 5,000 feet long. It begins at a proposed parking area at the eastern end of Carpinteria Avenue, near the Highway 101 on-ramp at the Highway 150 crossing. From there it travels southeast along the Highway terrace until it turns to the west to cross over the railroad tracks. After the railroad crossing the trail loops around a proposed bioswale before traveling southeast again along the coastal terrace until it reaches the Highway 101 off-ramp at the Rincon parking area. The trail route is shown on the attached preliminary landscape plan.

Existing Conditions: Most of the trail is located along either old roadways or old terraced road cut. The portion of the proposed trail from the railroad crossing to the Rincon parking lot is currently utilized as a trail for beach access.



Proposed Rincon Trail Looking West

Vegetation along the trail route consists primarily of coastal sage scrub and some fairly solid stands of Brewer's saltbrush (*Atriplex lentiformis breweri*); a common shrub scattered along sea bluffs and in canyons from Point Conception to Ventura (Smith, 1998). Nearly all of the vegetation in the above photo is Brewer's saltbrush. From Carpinteria Avenue to the railroad crossing the proposed trail route is vegetated primarily with our common coastal sage scrub, along with patches of introduced iceplant and eucalyptus trees. Most of the proposed trail south of the railroad crossing is covered with the Brewer's saltbrush, gradually shifting to coastal sage scrub as the trail nears the parking lot. Myoporum a non-native shrub/tree from New Zealand was noted in increasing numbers along the trail route by the Rincon parking area. Other weedy species have colonized the old road cuts, including black mustard, Russian thistle and fennel.

Proposed Trail: As seen in the attached Preliminary Landscape Plan the multi-use trail will be 10 to 15 feet wide, with an additional 5 to 10 feet needed for the trail shoulder, fencing and bio-swale. During construction of the trail an additional 20 to 40 feet of area may be impacted by the grading work for a total impact area of up to 60 feet depending on the trail location. However, because much of the trail has been located on old road cuts or existing terraces the construction impacts are expected to be less than anticipated.

Survey Results: A number of common animals were observed during the one field visit, including numerous western fence lizards, several side-blotched lizards, CA ground squirrels, brush rabbits, song sparrows, black phoebe, turkey vultures, two red-tailed hawks, mourning doves, house sparrows CA towhee, and rough-winged swallows. Animal sign indicated that woodrats utilize the coastal bluff along with grey fox, raccoons, opossum, and probably gopher snakes and king snakes. No sensitive plants or animals were observed during the site visit.

CA legless lizards (*Anniella pulchra*) are known to occur along the coast and are considered a species of special concern by the California Department of Fish and Game. These animals prefer moist warm loose soil with plant cover, and are known to occur in a variety of habitats including, sparsely vegetated areas of beach dunes, chaparral, pine-oak woodlands, desert scrub, sandy washes, and stream terraces with sycamores, cottonwoods, or oaks. Leaf litter under trees and bushes in sunny areas and dunes stabilized with bush lupine and mock heather often indicate suitable habitat. Can also be found by gently raking leaf litter under bushes and trees (Stebbins, 2003). It is unlikely that any legless lizards would be impacted by the trail construction since most of the trail is along old road cuts and there are no trees, but they may turn up especially near the proposed bioswale. Some preconstruction raking should be considered to confirm whether these unusual lizards are found within the trail corridor.

Conclusion: Common wildlife are abundant along the proposed Rincon trail. Use of the trail route by sensitive species is expected to be very limited since the trail does not cross any drainages or riparian corridors, and does not impact any wetlands. The CA legless lizard may be found in the area but as stated earlier some raking could be done to capture and relocate any animals prior to construction. There are also very few trees along the trail route that could be utilized for nesting by raptors.

Restoration efforts following the trail construction will restore the vegetation on either side of the trail. If any erosion control fabric is to be installed on the slopes, it is recommended that only

materials without any type of plastic netting be used. Biologists have recently documented instances where both lizards and snakes being caught in the plastic netting and have died.

References:

Smith, Clifton. 1998. A Flora of the Santa Barbara Region, California. Second Edition. Santa Barbara Botanic Garden & Capra press.

Stebbins, R. C. 2003. A Field Guide to Western Reptiles and Amphibians. Third Edition. Boston: Houghton Mifflin.



Carpinteria Rincon Trail

Attachment 2 Cultural Resources Survey

PHASE 1 CULTURAL RESOURCES SURVEY

RINCON TRAIL, CARPINTERIA

Prepared for:

Mr. Matt Roberts
Parks and Recreation
City of Carpinteria
5775 Carpinteria Avenue
Carpinteria, CA 93013

MARCH 2008

Prepared by:

David Stone, M.A., RPA
Ken Victorino, M.A., RPA

DUDEK

621 Chapala Street
Santa Barbara, CA 93101
Tel. (805) 963-0651

TABLE OF CONTENTS

<u>Section</u>	Page No.
1.0 INTRODUCTION	2
2.0 PROJECT DESCRIPTION	2
3.0 BACKGROUND RESEARCH	4
3.1 Prehistoric Setting	4
3.2 Historic Setting	5
3.3 Previous Research	6
4.0 FIELD INVESTIGATION	8
4.1 Methods	8
4.2 Results	9
5.0 CONCLUSIONS	9
6.0 RECOMMENDATIONS	11
6.0 REFERENCES	13

LIST OF FIGURES

Figure 1	Project Vicinity Map	3
----------	----------------------------	---

LIST OF APPENDICES

Appendix A	CCIC Records Search
Appendix B	NAHC Sacred Lands File Search

1.0 INTRODUCTION

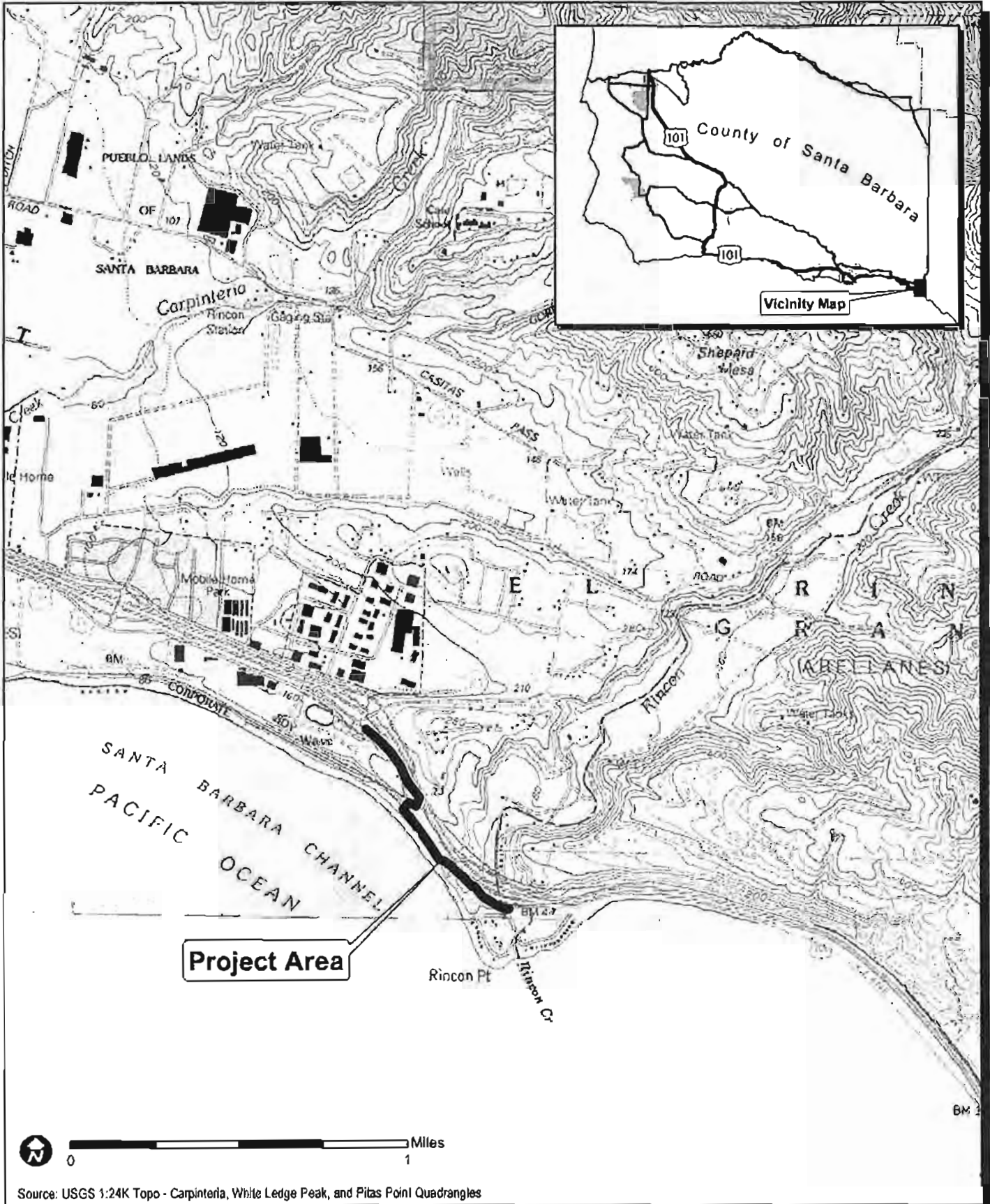
This report summarizes the results of a Phase 1 cultural resources investigation performed for the Rincon Trail in Carpinteria, California consistent with City of Carpinteria standards. The cultural resources investigation included an archaeological site records and literature search at the Central Coast Information Center, and an intensive surface reconnaissance of the proposed project area. The records search indicates that CA-SBA-1, the ethnohistoric/historic Chumash village of *Shuku*, is southwest of the proposed project area, near the mouth of Rincon Creek.

The 100-percent coverage, intensive Phase 1 pedestrian survey was performed in order to locate and evaluate all cultural resources within the proposed project area. Shovel scrapes were implemented to increase ground surface visibility during the survey. The resulting overall reliability of the survey is good. Several pieces of weathered shellfish were identified in a disturbed context along the Rincon Beach County Park parking lot, but with no discernable pattern or density. No other cultural material that may be associated with prehistoric occupation such as chipped and ground stone tools or bone was observed within the project improvement areas. The shellfish remains are not evident in sufficient density or diversity of species to provide information about occupational activities that may have occurred at this location. The weathered shellfish is not associated with any other artifacts or materials that suggest a specific prehistoric activity occurred there and it was found in a disturbed context along the Rincon Beach County Park parking lot. The proposed project area has also been subjected to previous disturbances associated with construction of Highway 101, the Union Pacific Railroad, and an asphalt path. The shellfish pieces do not provide the potential for yielding information important in prehistory and they lack integrity. Therefore, they are not considered a potentially significant archaeological resource. As a result, the proposed project would not have the potential for a significant impact on cultural resources and no other measures, including monitoring, are required.

2.0 PROJECT DESCRIPTION AND ENVIRONMENTAL SETTING

The proposed project includes building a hiking and biking trail from Carpinteria Avenue, to Rincon Beach County Park, and on to the Santa Barbara/Ventura County Line (Figure 1). The trail will begin at the eastern terminus of Carpinteria Avenue, cross over the Union Pacific Railroad on a bridge, and then continue along to the western-most end of Rincon Beach County Park. From there the trail will continue along the northern edge of the Rincon Beach County Park parking lot to Bates Road.

Soils in the project area are characterized as Xerorthents, cut and fill areas consisting of mechanically manipulated soils where the original profile is no longer discernible (USDA 1981).



Source: USGS 1:24K Topo - Carpinteria, White Ledge Peak, and Pitas Point Quadrangles

DUDEK

Rincon Trail
Project Vicinity Map

FIGURE
1

3.0 BACKGROUND RESEARCH

3.1 Prehistoric Setting

The local prehistoric chronology is divided into four major periods – Paleoindian, Early Period, Middle Period, and Late Period. It is generally accepted that humans entered the New World during the latter part of the Wisconsin glaciation between 40,000 and 20,000 years before present (B.P.). The earliest unquestioned evidence of human occupation in southern Santa Barbara County is dated to between 10,000 to 8,000 B.P. (Erlandson and Colten 1991). Paleoindian groups during this time focused on hunting Pleistocene megafauna, including mammoth and bison. Plants and smaller animals were undoubtedly part of the Paleoindian diet as well, and when the availability of large game was reduced by climatic shifts near the end of the Pleistocene, the subsistence strategy changed to a greater reliance on these resources.

Post-Pleistocene changes in climate and environment are reflected in the local archaeological record by approximately 8,000 B.P., the beginning of the Early Period, as defined by Chester King (1981, 1979, 1974). The Early Period of the Santa Barbara Channel mainland was originally defined by Rogers (1929), who called it the "Oak Grove" Period. The diagnostic feature of this period is the mano and metate milling stones, which were used to grind hard seeds such as sage for consumption. Toward the end of the Early Period, sea mammal hunting appears to have supplemented subsistence strategies (Glassow et al. 1990).

The Middle Period (3,350 to 800 B.P.) is characterized by larger and more permanent settlements, related to a generally wetter environment. Materials from Middle Period sites reflect a greater reliance on marine resources and include marine shells, fish remains, and fishhooks. A major shift in vegetable food exploitation occurred, as the mano and metate milling stones were replaced by stone mortars and pestles. This indicates a transition from seed gathering to oak tree acorn gathering and processing, a result of cooler temperatures and more expansive oak woodland habitats. Toward the end of this period, the plank canoe was developed, making ocean fishing and trade with the Channel Islands safer and more efficient (Arnold 1987). Terrestrial resources continued to be exploited as evidenced by the

presence of contracting-stemmed and corner-notched projectile points from Middle Period sites (Bamforth 1984).

The Late Period (800 to 150 B.P. or approximately A.D. 1150 to 1800) was a time of increased social and economic complexity. The increased number of permanent and semi-permanent villages clustered along the Santa Barbara Channel and on the Channel Islands, and the diversity of environmental site settings in which sites have been identified, indicates a substantial increase in prehistoric population. Intensification of terrestrial as well as marine resources occurred. Acorns continued to be processed, and land mammals were hunted with the bow and arrow, rather than exclusively by spear. Trade networks, probably controlled by village chiefs, expanded and played an important part in local Chumash culture, reinforcing status differences and encouraging craft specialization. Shell beads, found throughout the Early and Middle Periods, increased in number and variety, related to status and social value.

The protohistoric culture of the Chumash was terminated by the arrival of a Spanish expedition led by Gaspar de Portola in 1769. Chumash culture changed dramatically with the establishment of the Missions of Santa Barbara, Santa Ynez, and La Purísima.

3.2 Historic Setting

The historic occupation of the project vicinity can be divided into three settlement periods: the Mission Period (A.D. 1769 – 1830), the Rancho Period (ca. A.D. 1830 -1865), and the American Period (ca. A.D. 1865 – 1915). Construction of Mission Santa Barbara in 1786, Mission la Purísima Concepción in 1787, and Mission Santa Ynez in 1804, altered both the physical and cultural landscape of the region. The missions were the center of Spanish influence in the region and affected native patterns of settlement, culture, trade, industry, and agriculture. Following the secularization of the Missions by the Mexican Government in 1821, California became part of the Republic of Mexico.

Secularization of lands and a focus on cattle raising marked the Rancho Period, where large land grants of Mission lands were ceded to wealthy, prominent Spanish families. Native Americans continued to work as laborers on ranchos during this period. With California

statehood in 1850 and the advent of the American Period, farming and more intensive land uses steadily replaced cattle stock raising. Cattle ranching was substantially curtailed by a prolonged drought in the 1860s.

Since statehood, major forces of regional change during the last 150 years have been railroads, maritime shipping, agribusiness concerns, the oil industry, and the college institutions.

3.3 Previous Research

An archaeological site records and literature search of the California Historical Resources Information System at the Central Coast Information Center (CCIC) was conducted to identify all recorded investigations and archaeological sites within ¼-mile of the proposed project area (see Appendix A). The records search identified all known archaeological sites, historic resources, and previous cultural resource surveys within this distance. The CCIC records indicate that four archaeological sites have been recorded, and eight prior investigations have been undertaken within ¼-mile of the proposed project area.

CA-SBA-1, the ethnohistoric/historic Chumash village of *Shuku*, is located southwest of the proposed project area, at the mouth of Rincon Creek. CA-SBA-1 has been the subject of numerous archaeological investigations. However, there exists no synthesis of the accumulated information. In the 1870s Stephen Bowers noted that shell, bone, and fish scales covered at least 100 acres.

In the 1920s D.B Rogers (1929) noted that "the site of *Shuku* is of such wide extent and so diversified in topography that it is difficult to describe even roughly, and impossible to map without instruments of precision and assistants." Rogers goes on to describe the site as

situated upon a somewhat irregular "L" shaped tract, curving about the base of Rincon Mountain and lying between it and the sea... The eastern arm of the "L" shaped site is a flat, sandy strip of land only a few feet above high tide. It is perhaps one-fourth of a mile long by twenty rods [approximately three-fifths of a mile] wide... The northwestern arm of the elbow-shaped site is separated from the eastern arm by

Rincon Creek, which here expands into a pronounced estero, hemmed in from the sea by sand dunes. From this estero, the ancient sites rises sharply to the northwestward by a succession of benches, a total distance of one-half mile, the farthest portions capping high bluffs, probably one hundred feet in height, overlooking the sea on the one hand and the canyon of Rincon Creek on the other.

Villages of this size typically contain an abundant and diverse range of cultural material with high densities of both invertebrate and vertebrate remains. A Phase 2 Significance Assessment conducted east of Rincon Creek (SAIC 2004) recovered a diverse assemblage of cultural material including invertebrate and vertebrate remains, stone tool waste flakes, stone drills, projectile points, shell beads, shell fish hooks, glass beads, and bone tools. The invertebrate assemblage, consisting of at least 29 mutually exclusive taxa, indicated that the inhabitants of the village exploited a wide range of environmental habitats, including rocky shores, open sandy beaches, and bays/estuaries.

One of the prior investigations was conducted along the northern edge of the Rincon Beach County Park parking area for a cellular communications facility (SAIC 1996). The investigation consisted of a field survey and the excavation of three shovel test pits (STPs). The investigation concluded that the area was within the original boundary of the known prehistoric archaeological site CA-SBA-1. However, the STPs revealed that prehistoric artifacts were mixed with modern debris and it appeared that the area had been subject to extensive ground disturbances associated with the construction, and subsequent abandonment, of Highway 1. Therefore, the prehistoric cultural remains were not considered an important resource and no additional measures were required.

In addition to the CCIC site records search and literature review, a search of the Native American Heritage Commission's Sacred Land File was conducted in order to determine the location of any sacred and/or burial sites within the proposed project area (see Appendix B). The search did not indicate the presence of Native American cultural resources within the proposed project area.

4.0 FIELD INVESTIGATIONS

4.1 Methods

The Phase 1 cultural resources survey of the proposed improvement areas was conducted on Tuesday, March 4, 2008 and Tuesday, March 11, 2008 and utilized 5-meter (16-foot) transect intervals. Even though ground surface visibility ranged from poor (less than 10 percent) to excellent (90 to 100 percent), in general, the ground surface visibility was fair (10 to 50 percent). The resulting overall reliability of the survey is good.

The first portion of the trail, from the eastern terminus of Carpinteria Avenue to the Union Pacific Railroad, traverses a steep engineered slope overlooking Highway 101. The slope, cut during construction of the highway, was covered with ice plant and ground surface visibility was generally poor (less than 10 percent). However, bare spots allowed examination of the ground surface. These bare spots often revealed bedrock indicating that the engineered slope had been cut deeply enough to encounter the underlying bedrock. No prehistoric or historic materials were observed during the survey of the first portion of the trail, from the eastern terminus of Carpinteria Avenue to the Union Pacific Railroad.

The trail will cross the Union Pacific Railroad in an area that consists of engineered slopes cut during construction of the railroad.

The second portion of the trail, from the Union Pacific Railroad to the western-most end of Rincon Beach County Park, follows an existing trail. The trail, graded flat during construction of the trail, is now mostly dirt. However, the original asphalt still remains in some areas. Ground surface visibility along the existing trail ranged from good (50 to 90 percent) to excellent (90 to 100 percent). A few small pieces of weathered shellfish were observed near the metal gate at the western-most end of Rincon Beach County Park. The weathered shellfish was identified in an area of sandy soil that appears to have eroded from above. No other cultural material that may be associated with prehistoric occupation such as chipped and ground stone tools or bone was observed.

The third portion of the trail, from the western-most end of Rincon Beach County Park to Bates Road, is located along the northern edge of the Rincon Beach County Park parking lot. Ground surface visibility along the third portion was limited by dense vegetation and was generally poor (less than 10 percent). Shovel scrapes were implemented where feasible along the third portion to increase ground surface visibility during the survey. Previous grading associated with the construction of the parking lot has impacted most of the third portion. A few small pieces of weathered shellfish were observed in a disturbed context along the northern edge of the Rincon Beach County Park parking lot. No other cultural material that may be associated with prehistoric occupation such as chipped and ground stone tools or bone was observed.

4.2 Results

Several small pieces of weathered shellfish were identified in disturbed contexts near the metal gate at the western-most end of Rincon Beach County Park and along the northern edge of Rincon County Park parking lot, but with no discernable pattern or density. The shellfish, mainly unidentifiable Veneridae (Venus clams), was identified in very low densities. This shellfish assemblage contrasts with the wide range of species exploited by the inhabitants of CA-SBA-1. No other cultural material that may be associated with prehistoric occupation such as chipped and ground stone tools or bone was observed within the project area.

5.0 CONCLUSIONS

The quality of information from archaeological site deposits is related to the intactness or integrity of the soil in which the materials are found. Therefore, integrity is a critical factor in establishing the significance of archaeological deposits. Assuming an archaeological deposit is found to have intact soil integrity, the following factors are used to determine qualitatively the relative significance of deposits. The California Environmental Quality Act (CEQA) Guidelines Section 15064.5.a3 criteria states:

Any object, building, structure, site, area, place, record, or manuscript which a lead agency determines to be historically significant or significant in the architectural,

engineering, scientific, economic, agricultural, educational, social, political, military, or cultural annals of California may be considered to be an historical resource, provided the lead agency's determination is supported by substantial evidence in light of the whole record. Generally, a resource shall be considered by the lead agency to be "historically significant" if the resource meets the criteria for listing on the California Register of Historical Resources (Pub. Res. Code SS5024.1, Title 14 CCR, Section 4852) including the following:

- a. is associated with events that have made a significant contribution to the broad patterns of California's history and cultural heritage;
- b. is associated with the lives of persons important in our past;
- c. embodies the distinctive characteristics of a type, period, region, or method of construction, or represents the work on an important creative individual, or possesses high artistic values; or
- d. has yielded, or may be likely to yield, information important in prehistory or history.

Criterion "d" is most often used to evaluate the significance of prehistoric cultural remains. The ability of an archaeological site deposit to yield information important in prehistory is framed in terms of the data available to address research questions about the past. The goal of collecting information from one archaeological site is to be able to contribute to our understanding of regional cultural adaptations that may have changed through time due to environmental (i.e., increased or decreases temperatures, rainfall, etc.) and/or social (i.e., increased population, competition for food resources, status, warfare, etc.) pressures. Therefore, the remains that are recovered from a particular archaeological site are compared to the existing information available from neighboring sites to determine if they can help explain patterns of behavior over a larger area.

The current Phase 1 Cultural Resources Survey identified several pieces of weathered shellfish within the proposed project area, in disturbed contexts near the metal gate at the westernmost end of Rincon Beach County Park and along the northern edge of the Rincon Beach County Park parking lot. Particularly since the shellfish pieces were identified on the ground

surface in areas that have been impacted by previous grading, their location is considered to be the result of modern activities and soil movement (i.e., grading associated with construction of Highway 101, the Union Pacific Railroad, an asphalt path, and the Rincon Beach County Park parking lot), and not the discard by prehistoric peoples. The recovered shellfish, mainly unidentifiable Veneridae (Venus clams), lacked the density and diversity documented at CA-SBA-1. No other cultural material associated with prehistoric occupation including stone tools, animal bone, or ground stone implements were identified. Even if the shellfish were *in situ* (i.e., in its original depositional location) and associated with prehistoric activity, the shell is not of sufficient density or diversity to provide important information to explain and understand the prehistoric occupation of coastal areas of Santa Barbara County, particularly when compared to the substantial CA-SBA-1 deposits. Therefore, the pieces of shellfish observed in disturbed contexts within the proposed project area are not considered a potentially significant archaeological resource under CEQA Guidelines Section 15064.5.a.3, and Santa Barbara County Cultural Resources Guidelines.

As a result, no potentially important archaeological resources are located within the proposed improvement areas. Therefore, future ground disturbing construction activities within the proposed project area would not have the potential to significantly impact cultural resources.

This conclusion is confirmed by the previous SAIC investigation (1996) that was conducted along the northern edge of the Rincon Beach County Park parking lot for a cellular communications facility. The investigation concluded that prehistoric artifacts were mixed with modern debris and the area had been subject to extensive ground disturbances associated with the construction, and subsequent abandonment, of Highway 1. Therefore, the prehistoric cultural remains were not considered an important resource and no additional measures were required.

6.0 RECOMMENDATIONS

As no potentially significant cultural resources were identified within the proposed improvement areas, the proposed project would not have the potential for a significant impact on cultural resources. Therefore, no further measures such as construction monitoring are

necessary. The following is recommended in the unlikely event that potentially significant cultural remains are encountered during construction, consistent with guidance provided in CEQA Guidelines Section 15064.5:

1. In the unlikely event that potentially significant cultural materials are encountered during construction, grading should be temporarily redirected and/or suspended until a qualified archaeologist and local Chumash representative are retained to evaluate the find, including mapping and collecting any diagnostic (time-sensitive) artifacts, consistent with City of Carpinteria standards.

The above recommended measure would ensure that the low potential for impacts to unknown cultural resources to occur during project construction activities would be addressed consistent with City of Carpinteria standards.

7.0 REFERENCES

- Arnold, J.E. 1987. Craft Specialization in the Prehistoric Channel Islands, California. *University of California Publications in Anthropology*, No. 18. Berkeley.
- Bamforth, D.B. 1984. Analysis of Chipped Stone Artifacts. In *Archaeological Investigations on the San Antonio Terrace, Vandenberg Air Force Base, California, in Connection with MX Facilities Construction*. Chambers Consultants and Planners. Submitted to U.S. Army Corps of Engineers, Los Angeles District.
- Erlandson, Jon M., and Roger Colten. 1991. Hunter-Gatherers of Early Holocene Coastal California. *Perspectives in California Archaeology, Volume I*. Edited by Jon M. Erlandson and Roger Colten. Institute of Archaeology, University of California, Los Angeles.
- Glassow, M.A., with contributions by Jeanne E. Arnold, G.A. Batchelder, D.T. Fitzgerald, B. Glenn, D.A. Guthrie, D.L. Johnson, and P.L. Walker. 1990. *Archaeological Investigations on Vandenberg Air Force Base in Connection with the Development of Space Transportation System Facilities, Volume I*.
- King, Chester. 1981. *The Evolution of Chumash Society: A Comparative Study of Artifacts Used in Social System Maintenance in the Santa Barbara Channel Region before A.D. 1804*. Ph.D. dissertation, Department of Anthropology, University of California, Davis.
- _____. 1979. Beads and Selected Ornaments. In *Final Report: Archaeological Studies at Oro Grande, Mojave Desert, California*. Edited by C. Rector, J. Swenson, and P. Wilke. Archaeological Research Unit, University of California, Riverside.
- _____. 1974. The Explanation of Differences and Similarities Among Beads Used in Prehistoric and Early Historic California. In *Antap, California Indian Political and Economic Organization*. Edited by L.J. Bean and T.F. King. *Ballena Press Anthropological Papers* 2: 75-92.
- Rogers, David Banks. 1929. *Prehistoric Man of the Santa Barbara Coast*. Santa Barbara Museum of Natural History.
- Science Applications International Corporation (SAIC). 1996. Phase 1 Cultural Resources Investigation Cellular Communications Facility, AT&T Wireless Services, Bates Road, Rincon Point, California.

Rincon Trail - Cultural Resources Survey

_____. 2004. Phase 2 Archaeological Significance Assessment, CA-SBA-1/CA-VEN-62, Rincon Point Septic to Sewer Project, Santa Barbara and Ventura Counties, California.

United States Department of Agriculture (USDA). 1981. Soil Survey of Santa Barbara County, California (South Coastal Part).

APPENDIX A
CCIC Records Search

CENTRAL COAST INFORMATION CENTER

California
Archaeological
Inventory



SAN LUIS OBISPO AND
SANTA BARBARA COUNTIES

Department of Anthropology
University of California, Santa Barbara
Santa Barbara, CA 931063210
(805) 893-2474
FAX (805) 893-8707

3/10/2008

Ken Victorino
Dudek and Associates
621 Chapala St.
Santa Barbara, CA 93101

Dear Mr. Victorino,

Enclosed are the results of the record search you requested for the Rincon Trail, Carpinteria (Project No. 5734-05). Our records were searched for all known archaeological sites, historic resources, and previous cultural resource surveys within a quarter mile radius of the project area.

In this search, four archaeological site(s) and eight previous cultural resource survey(s) were found. The survey locations were mapped onto portions of the White Ledge Peak quad(s). A bibliography of these surveys is included. A search of the inventories for the State Historic Property Data Files, National Register of Historic Places, National Register of Determined Eligible Properties, California Historical Landmarks, California Points of Historic Interest, California OHP Archaeological Determinations of Eligibility, and the Caltrans State and Local Bridge Surveys yielded one property evaluation(s) within the search radius.

According to our records, the project area has not been surveyed. Therefore a cultural resource survey is recommended.

Please contact me if you have any questions about this search.

Sincerely,



Amy Gusick
Assistant Coordinator

APPENDIX B
NAHC Sacred Lands File Search

STATE OF CALIFORNIA

Arnold Schwarzenegger, Governor

**NATIVE AMERICAN HERITAGE
COMMISSION**915 CAPITOL MALL, ROOM 364
SACRAMENTO, CA 95814
(916) 653-4882
Fax (916) 657-5390

March 7, 2008

Ken Victorino
DUDEK
621 Chapala Street
Santa Barbara, CA 93101

Sent by Fax: 805-963-2074

Number of Pages: 3

Re: Proposed Rincon Trill project number 8734-05; Santa Barbara County.

Dear Mr. Victorino:

A record search of the sacred lands file has failed to indicate the presence of Native American cultural resources in the immediate project area. The absence of specific site information in the sacred lands file does not indicate the absence of cultural resources in any project area. Other sources of cultural resources should also be contacted for information regarding known and recorded sites.

Enclosed is a list of Native Americans individuals/organizations who may have knowledge of cultural resources in the project area. The Commission makes no recommendation or preference of a single individual, or group over another. This list should provide a starting place in locating areas of potential adverse impact within the proposed project area. I suggest you contact all of those indicated, if they cannot supply information, they might recommend others with specific knowledge. If a response has not been received within two weeks of notification, the Commission requests that you follow-up with a telephone call to ensure that the project information has been received.

If you receive notification of change of addresses and phone numbers from any of these individuals or groups, please notify me. With your assistance we are able to assure that our lists contain current information. If you have any questions or need additional information, please contact me at (916) 653-4040.

Sincerely,

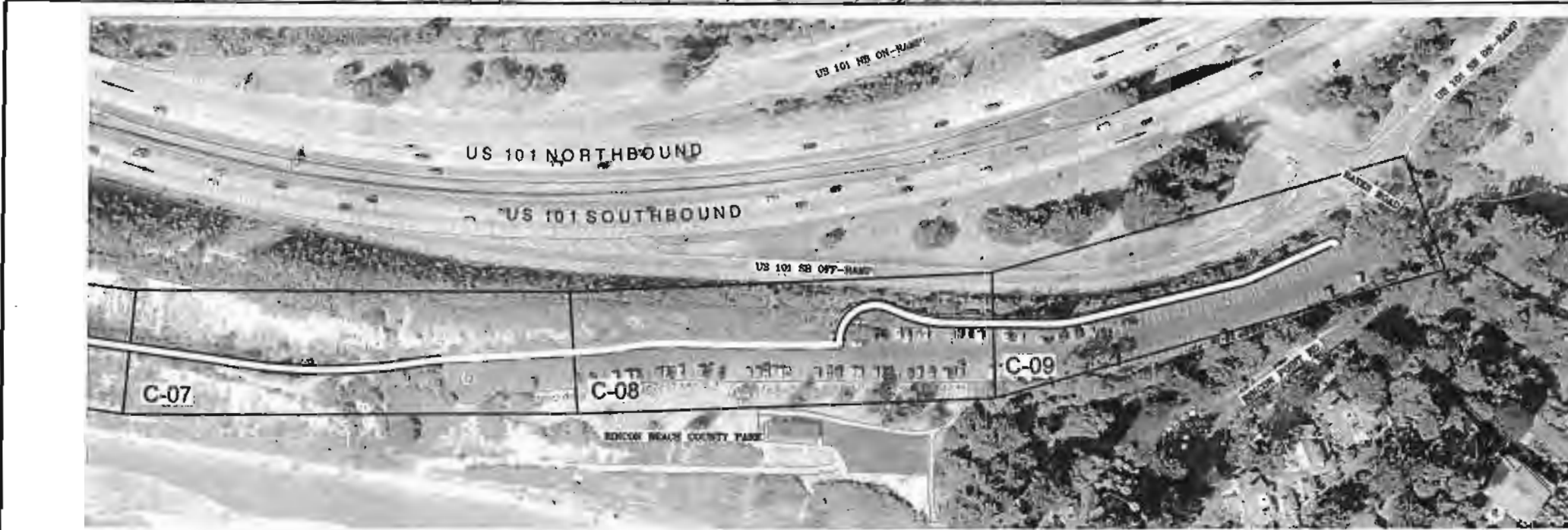
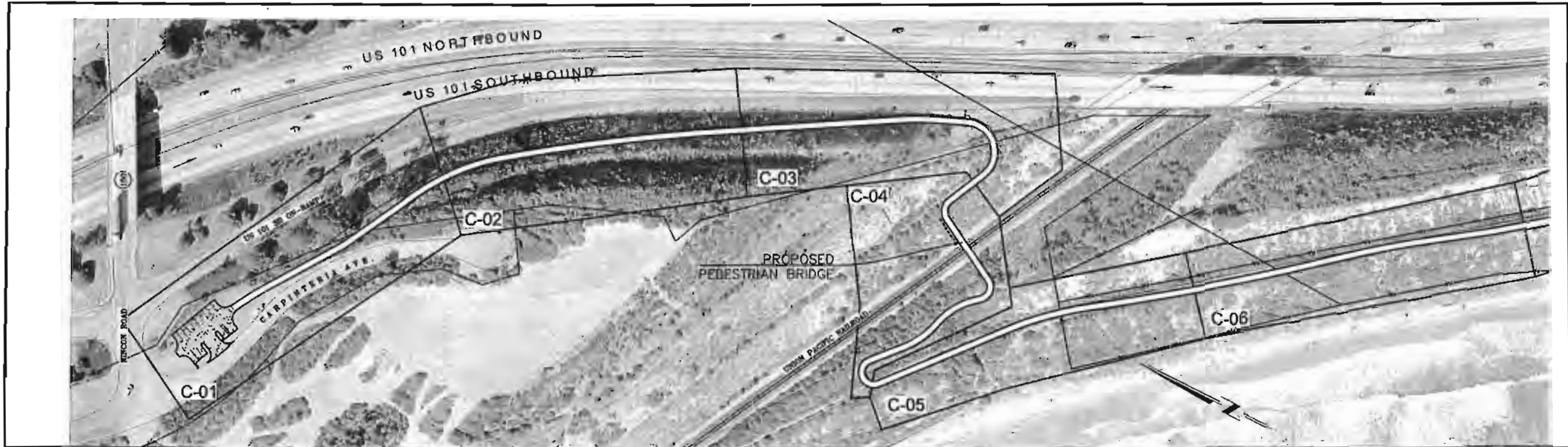
A handwritten signature in cursive script that reads "Katy Sanchez".

Katy Sanchez
Program Analyst



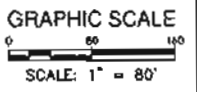
Carpinteria Rincon Trail

Attachment 3
Preliminary Plans



KEY MAP
SCALE: 1"=80'

NOTE:
PROPERTY LINES ARE SHOWN PER INFORMATION FROM BENNER & CARPENTER. FURTHER RESEARCH IS NEEDED TO VERIFY PROPERTY LINES AND OWNERSHIP.

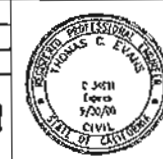


G-02

REV. NO.	DATE	DESCRIPTION	APPVD.	DATE
REVISIONS				

DESIGN: TCE & GB
DRAWN: GB & NSL
CHECKED: TCE

LINE IS 3/16" INCHES AT FULL SIZE
IF NOT 3" - SCALE ACCORDINGLY



DUDEK
Dudek & Associates, Inc.
421 Chapel Street, Santa Barbara, CA
93101 805.963.0651 Fax 805.963.2074

PRELIMINARY DESIGN
NOVEMBER 3, 2008
NOT FOR CONSTRUCTION

CITY OF CARPINTERIA PARKS & RECREATION	
RINCON BEACH MULTI-USE TRAIL	
LAYOUT MAP	
PRELIMINARY	
SCALE: HORIZONTAL: AS NOTED VERTICAL: AS NOTED	DATE: 11/03/08 SHEET 2 of 12



VAN ATTA Associates, Inc.
 landscape architecture • planning
 2000 Main Street, Suite 200
 San Jose, CA 95128
 Tel: 415.735.1100

Rincon Trail

Carpenteria, CA

March, 2008

Drawn By:
 NAME: _____ DATE: _____
 CO. NO. _____

Revisions
 # NAME DATE

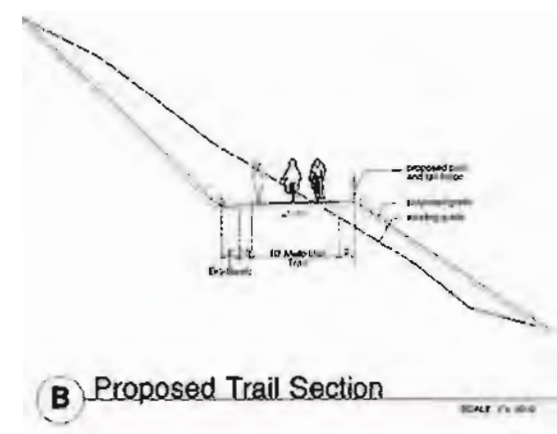
scale: as noted
 © 2008 Van Atta Associates, Inc.
 This design, when used, shall be the property of Van Atta Associates, Inc.
 and shall not be reproduced, stored in a retrieval system, or transmitted in any form or by any means, electronic, mechanical, photocopying, recording, or by any information storage and retrieval system, without the written permission of Van Atta Associates, Inc.



Preliminary Plan
 Landscape



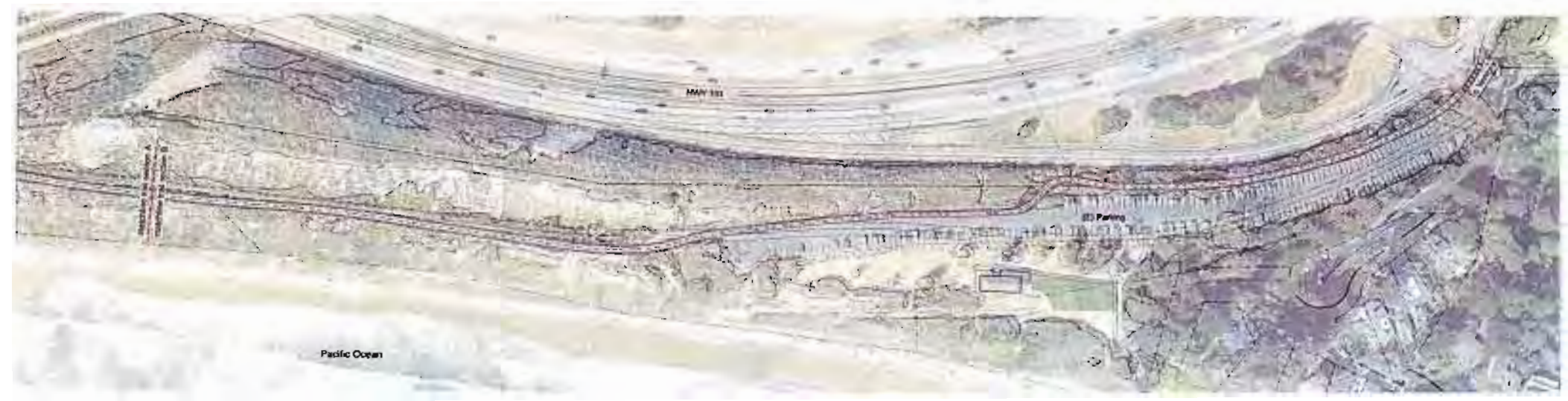
B Proposed Trail From New Parking Lot to Rail Road Bridge Crossing



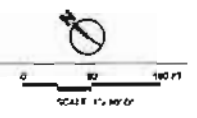
B Proposed Trail Section

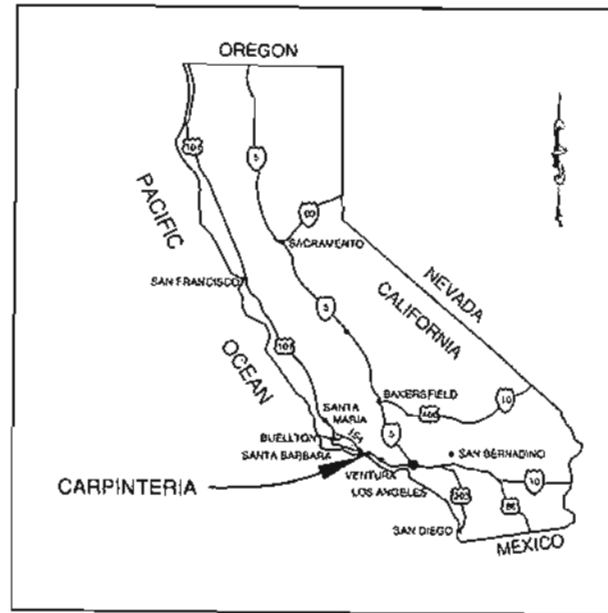


A Proposed Trail Section



A Proposed Trail From Rail Road Bridge Crossing To HWY 101 On-Ramp

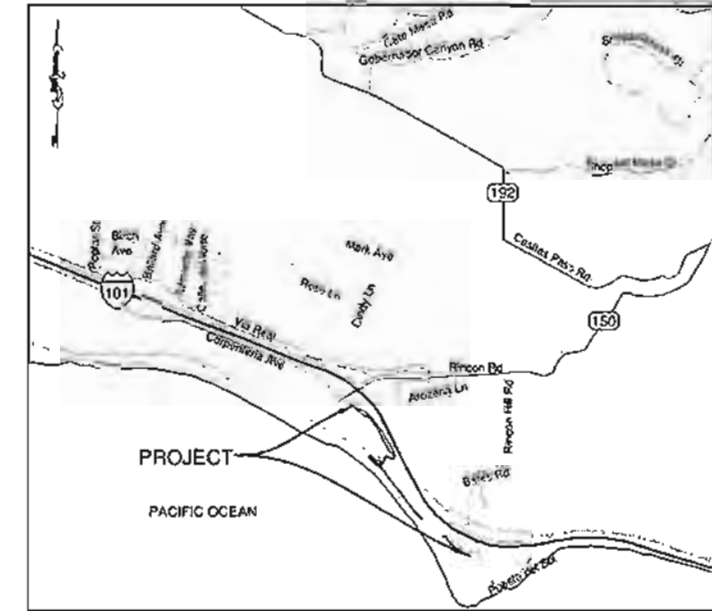




LOCATION MAP
NOT TO SCALE

CITY OF CARPINTERIA, CALIFORNIA

RINCON BEACH MULTI-USE TRAIL



VICINITY MAP
NOT TO SCALE

SHEET INDEX

SHEET NO.	DESCRIPTION
1	G-1 TITLE SHEET, LEGEND, SYMBOLS, ABBREVIATIONS
2	G-3 LAYOUT MAP
3	C-1 PLAN AND PROFILE (STA 0+00 - STA 5+00)
4	C-2 PLAN AND PROFILE (STA 5+00 - STA 10+00)
5	C-3 PLAN AND PROFILE (STA 10+00 - STA 15+00)
6	C-4 PLAN AND PROFILE (STA 15+00 - STA 20+00)
7	C-5 PLAN AND PROFILE (STA 20+00 - STA 25+00)
8	C-6 PLAN AND PROFILE (STA 25+00 - STA 31+00)
9	C-7 PLAN AND PROFILE (STA 31+00 - STA 36+00)
10	C-8 PLAN AND PROFILE (STA 36+00 - STA 42+00)
11	C-9 PLAN AND PROFILE (STA 42+00 - STA 48+49.62)
12	C-10 CROSS SECTIONS STA 4+00 AND STA 9+00 & EARTHWORK VOLUME SUMMARY

ABBREVIATIONS

AC	ASPHALTIC CONCRETE	HORIZ	HORIZONTAL
BC	BEGIN CURVE	PVI	POINT OF VERTICAL INTERSECTION
BVCE	BEGIN VERTICAL CURVE STATION	STA	STATION
CL	CENTERLINE	TYP	TYPICAL
CY	CUBIC YARD	VC	VERTICAL CURVE
DG	DIRT GROUND	VERT	VERTICAL
EC	END CURVE	W/	WITH
EVCE	END VERTICAL CURVE STATION		
EX	EXISTING		
FCL	CHAIN LINK FENCE		

UTILITIES

ELECTRIC:	SOUTHERN CALIF. EDISON	983-3671
TELEPHONE:	GTE	969-5598
CABLE TV:	COX COMMUNICATION	683-7751
GAS:	SOUTHERN CALIF. GAS CO.	984-7851
WATER:	CARPINTERIA VALLEY WATER DISTRICT	684-2818
SEWER:	CARPINTERIA SANITARY DISTRICT	684-7214

BENCH MARK

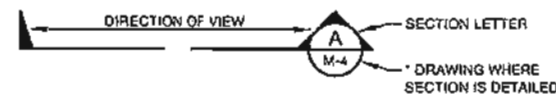
95-2 & 95-3, CARPINTERIA AVE, CARPINTERIA, CA

LEGEND

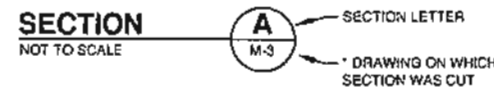
DESCRIPTION	SYMBOL
EXISTING TREE OR BRUSH	
EXISTING SPOT ELEVATION	
SURVEY CONTROL TARGET	
EXISTING CONTOUR	
EXISTING RAILROAD	
EXISTING FENCE	
PROPOSED RETAINING WALL	
PROPOSED FENCE	
RIGHT OF WAY	
PROPERTY LINE	
DAYLIGHT	

CROSS REFERENCING SYSTEM

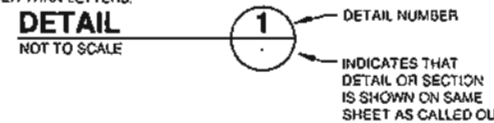
(1) A SECTION CUT ON DRAWING M-3 IS IDENTIFIED AS FOLLOWS:



(2) ON DRAWING M-4 THE SECTION IS IDENTIFIED AS FOLLOWS:



(3) DETAILS ARE CROSS REFERENCED IN A SIMILAR MANNER TO THAT OF SECTIONS EXCEPT DETAILS ARE ASSIGNED NUMBERS RATHER THAN LETTERS.



AERIAL PHOTO 07/07/07 BY GOLDEN STATE AERIAL, INC
SAN LUIS OBISPO, CA.

REV. NO.	DATE	DESCRIPTION	APPVD.	DATE

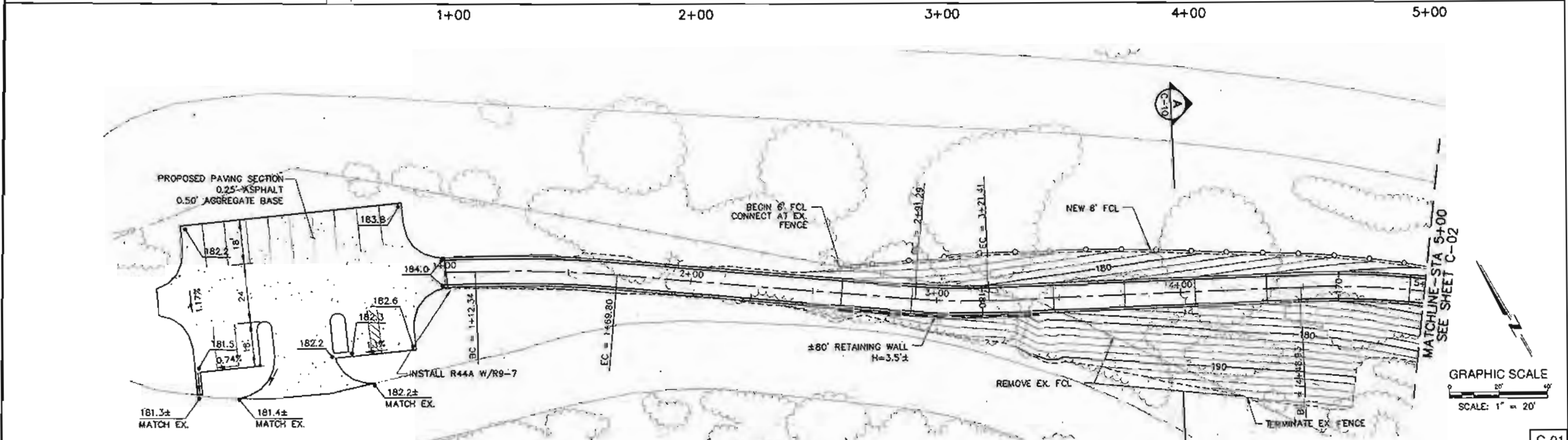
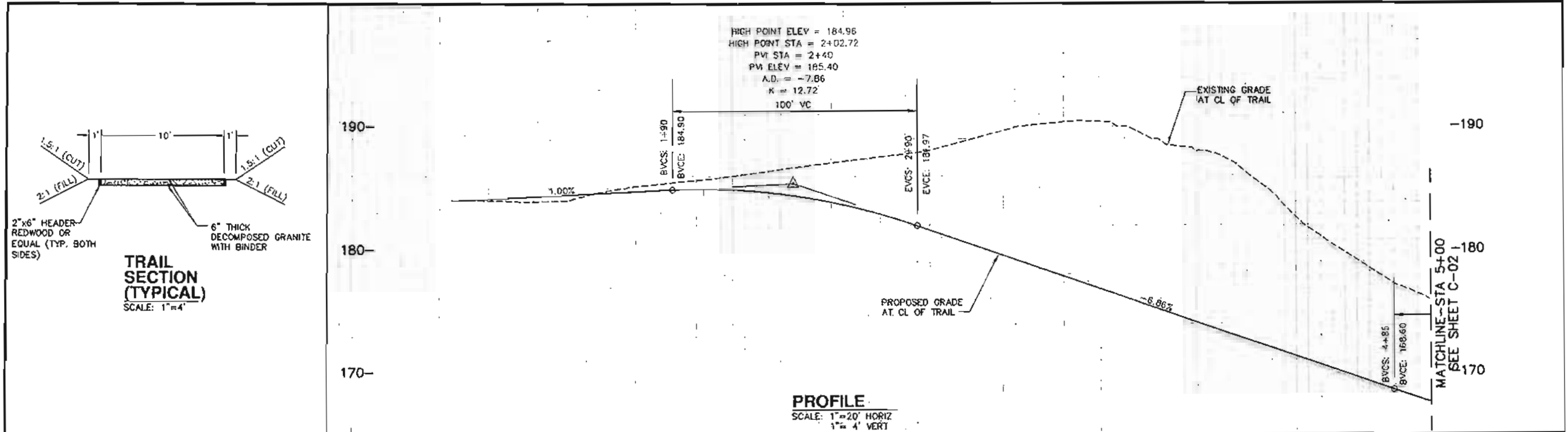
DESIGN:	TCE & GB
DRAWN:	GB & NSL
CHECKED:	TCE



DUDEK
Dudek & Associates, Inc.
621 Chapala Street, Santa Barbara, CA
93101 805.963.0651 Fax 805.963.2874

PRELIMINARY DESIGN
NOVEMBER 3, 2008
NOT FOR CONSTRUCTION

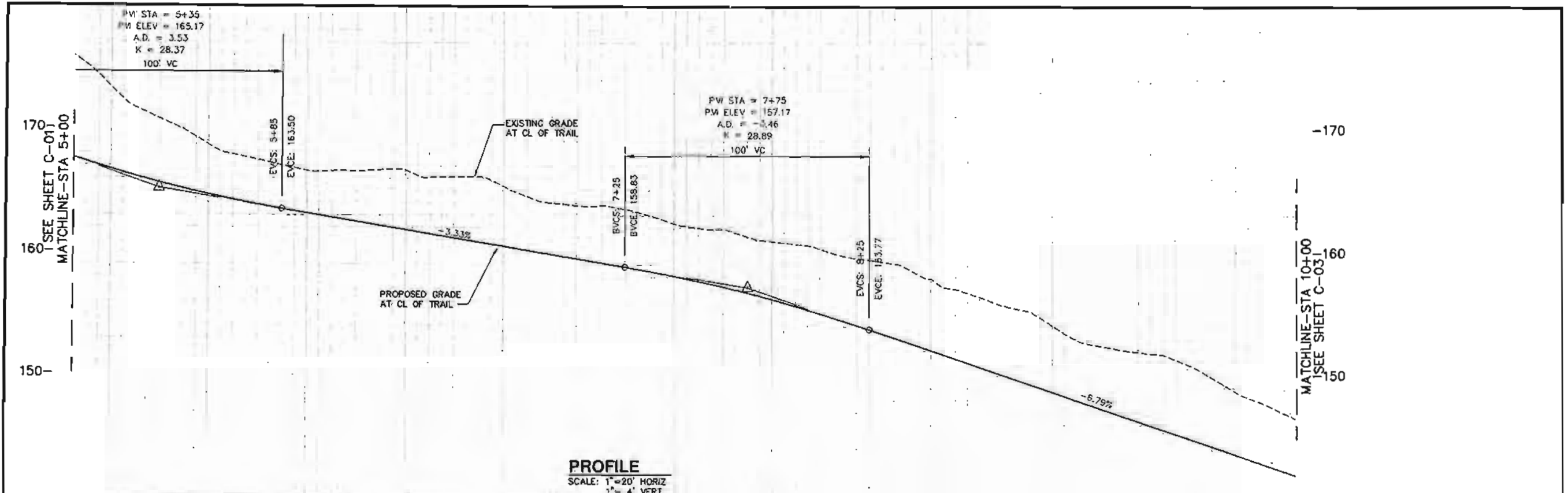
G-01
CITY OF CARPINTERIA PARKS & RECREATION
RINCON BEACH MULTI-USE TRAIL
TITLE SHEET
PRELIMINARY
SCALE: HORIZONTAL: AS NOTED DATE: 11/03/08 SHEET 1 OF 12



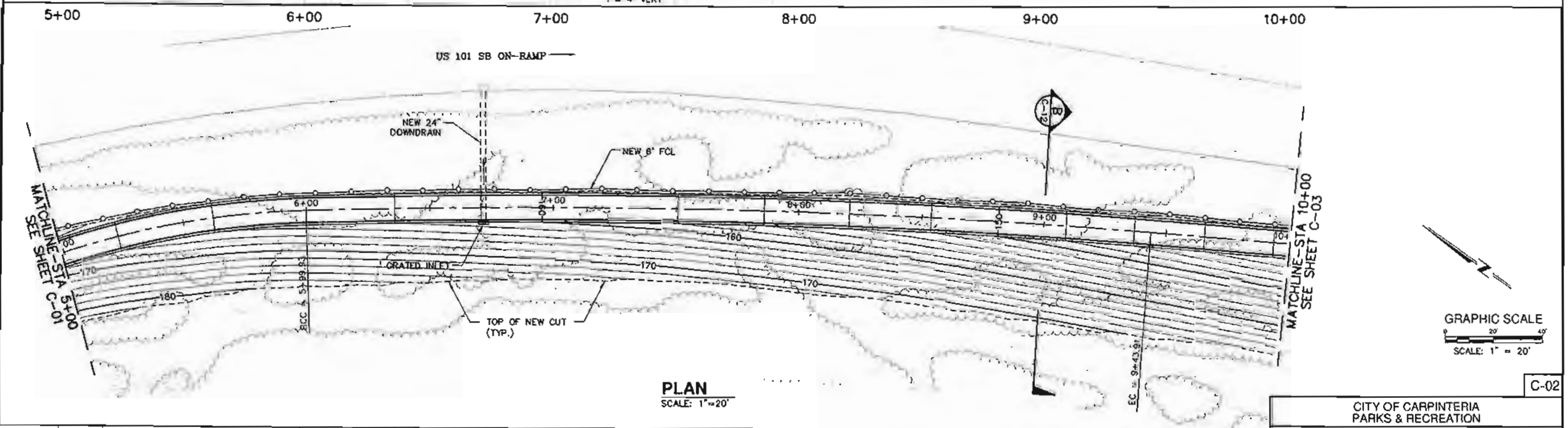
TRAIL SECTION (TYPICAL) SCALE: 1"=4'		PROFILE SCALE: 1"=20' HORIZ 1"=4' VERT	
PLAN SCALE: 1"=20'		GRAPHIC SCALE SCALE: 1"=20'	
DUDEK Dudek & Associates, Inc. 621 Chapala Street, Santa Barbara, CA 93101 805.963.0651 Fax 805.963.2074		PRELIMINARY DESIGN NOVEMBER 3, 2008 NOT FOR CONSTRUCTION	
CITY OF CARPINTERIA PARKS & RECREATION RINCON BEACH MULTI-USE TRAIL PLAN AND PROFILE: STA 0+00 TO STA 5+00 PRELIMINARY		SCALE: HORIZONTAL: AS NOTED DATE: 11/03/08 SHEET 3 of 12 VERTICAL: AS NOTED	

REV. NO.	DATE	DESCRIPTION	APPVD.	DATE
REVISIONS				

DESIGN: TCE & GB	
DRAWN: GB & NSL	
CHECKED: TCE	



PROFILE
SCALE: 1" = 20' HORIZ
1" = 4' VERT



PLAN
SCALE: 1" = 20'

REV. NO.	DATE	DESCRIPTION	APPVD.	DATE
REVISIONS				

DESIGN: TCE & DB
DRAWN: GB & NSL
CHECKED: TCE

LINE IS 2 INCHES AT PAPER SIZE IF NOT 2" = SCALE ACCORDINGLY

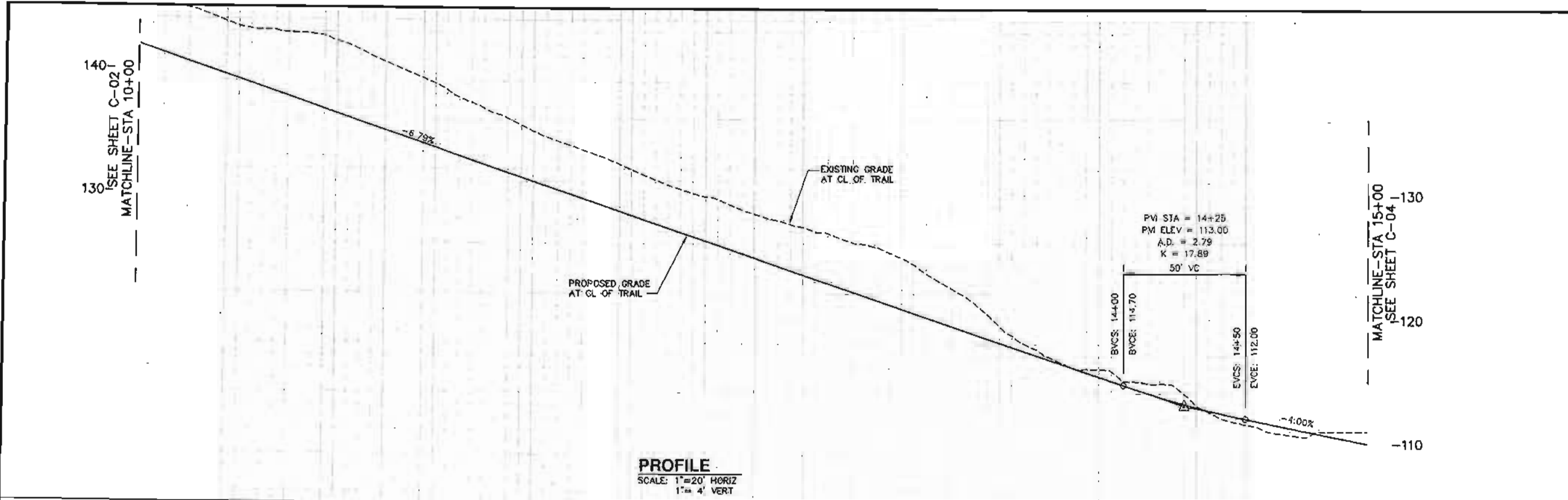
DUDEK
Dudek & Associates, Inc.
621 Chapala Street, Santa Barbara, CA
93101 805.963.0651 Fax 805.963.2074

PRELIMINARY DESIGN
NOVEMBER 3, 2008
NOT FOR CONSTRUCTION

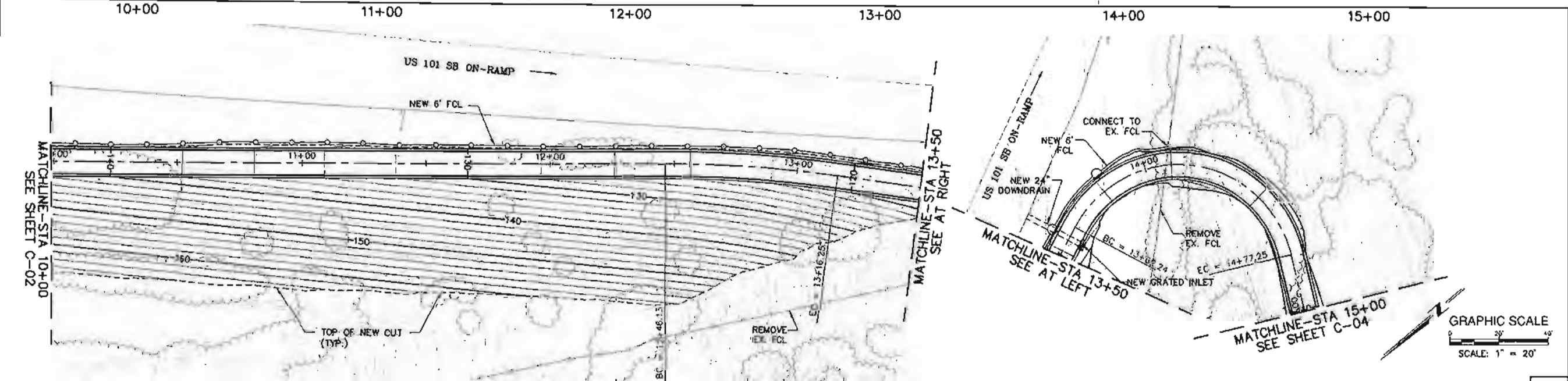
CITY OF CARPINTERIA
PARKS & RECREATION
RINCON BEACH MULTI-USE TRAIL
PLAN AND PROFILE:
STA 5+00 TO STA 10+00
PRELIMINARY

SCALE: HORIZONTAL: AS NOTED DATE: 11/03/08 SHEET 4 of 12
VERTICAL: AS NOTED

C-02



PROFILE
SCALE: 1"=20' HORIZ
1"= 4' VERT

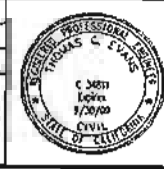


PLAN
SCALE: 1"=20'

C-03

REV. NO.	DATE	DESCRIPTION	APPVD.	DATE
REVISIONS				

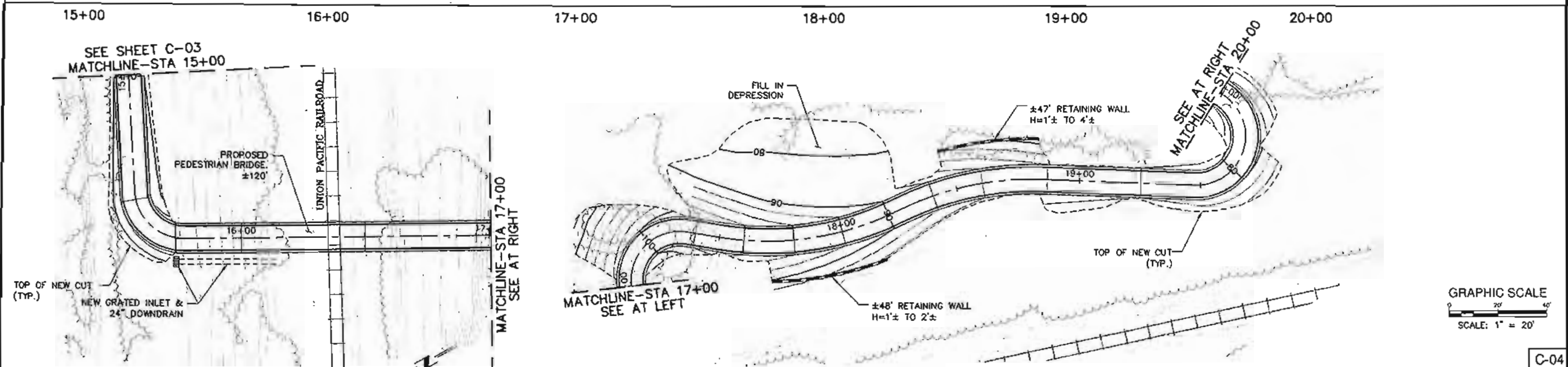
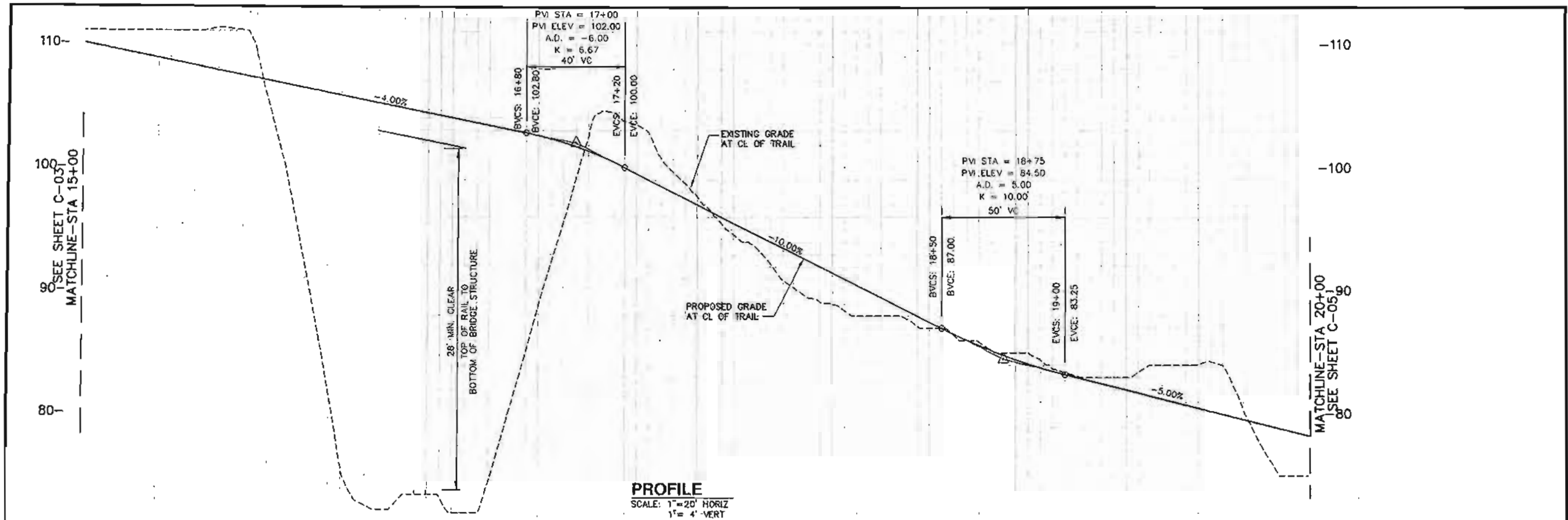
DESIGN: TCE & GB
DRAWN: CB & HSL
CHECKED: TCE



DUDEK
Dudek & Associates, Inc.
621 Chapala Street, Santa Barbara, CA
93101 805.963.0651 Fax 805.963.2074

PRELIMINARY DESIGN
NOVEMBER 3, 2008
NOT FOR CONSTRUCTION

CITY OF CARPINTERIA
PARKS & RECREATION
RINCON BEACH MULTI-USE TRAIL
PLAN AND PROFILE:
STA 10+00 TO STA 15+00
PRELIMINARY
SCALE HORIZONTAL: AS NOTED DATE: 11/03/08 SHEET 5 of 12
VERTICAL: AS NOTED



REV. NO.	DATE	DESCRIPTION	APPVD.	DATE
REVISIONS				

DESIGN: TCE & GB
DRAWN: GB & NSL
CHECKED: TCE

LINE IS 2 INCHES AT FULL SIZE IF NOT 2" - SCALE ACCORDINGLY

DUDEK
Dudek & Associates, Inc.
621 Chapala Street, Santa Barbara, CA
93101 805.963.0651 Fax 805.963.2074

REGISTERED PROFESSIONAL ENGINEER
THOMAS C. EVANS
C. 3481
Expires 9/30/09
CIVIL
STATE OF CALIFORNIA

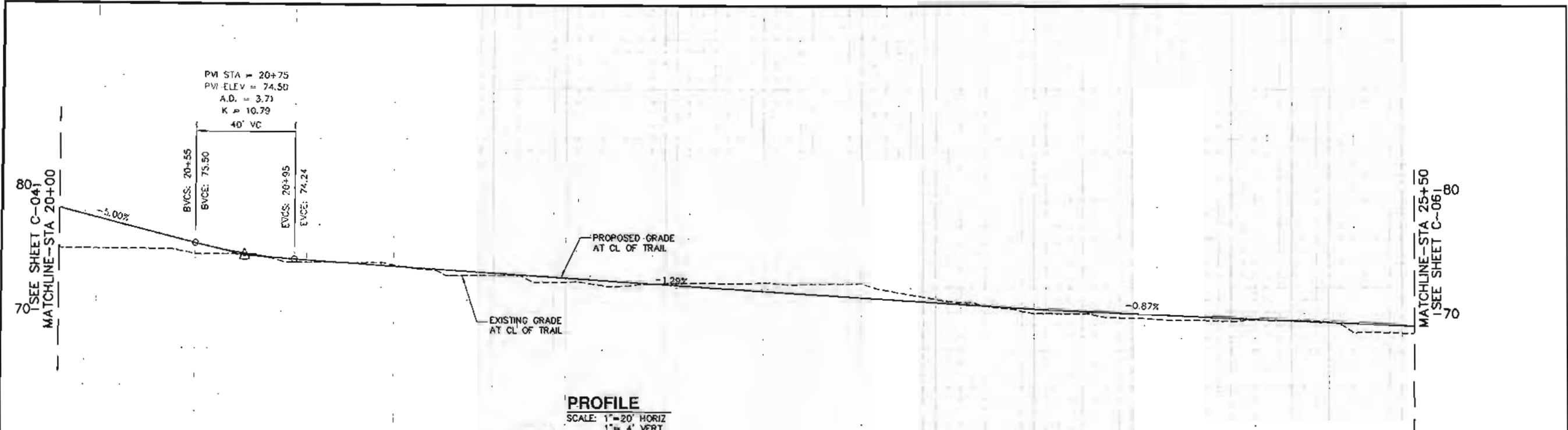
GRAPHIC SCALE
SCALE: 1" = 20'

C-04

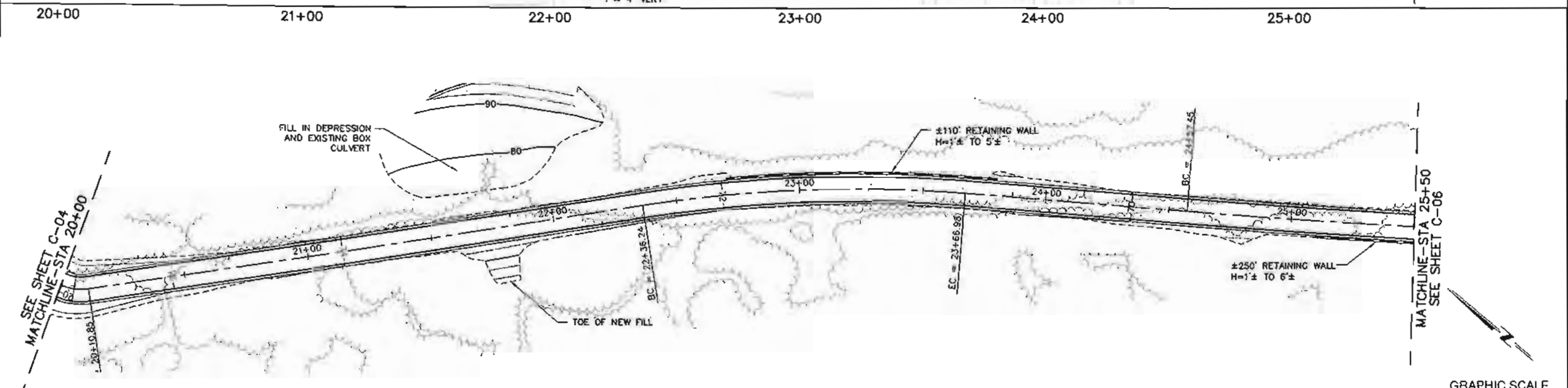
CITY OF CARPINTERIA
PARKS & RECREATION
RINCON BEACH MULTI-USE TRAIL
PLAN AND PROFILE:
STA 15+00 TO STA 20+00
PRELIMINARY

PRELIMINARY DESIGN
NOVEMBER 3, 2008
NOT FOR CONSTRUCTION

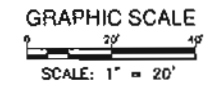
SCALE: HORIZONTAL: AS NOTED DATE: 11/03/08 SHEET 6 of 12
VERTICAL: AS NOTED



PROFILE
SCALE: 1" = 20' HORIZ
1" = 4' VERT



PLAN
SCALE: 1" = 20'



C-05

REV. NO.	DATE	DESCRIPTION	APPVD.	DATE
REVISIONS				

DESIGN: YCE & GB
DRAWN: OB & NSL
CHECKED: TCE

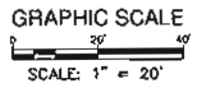
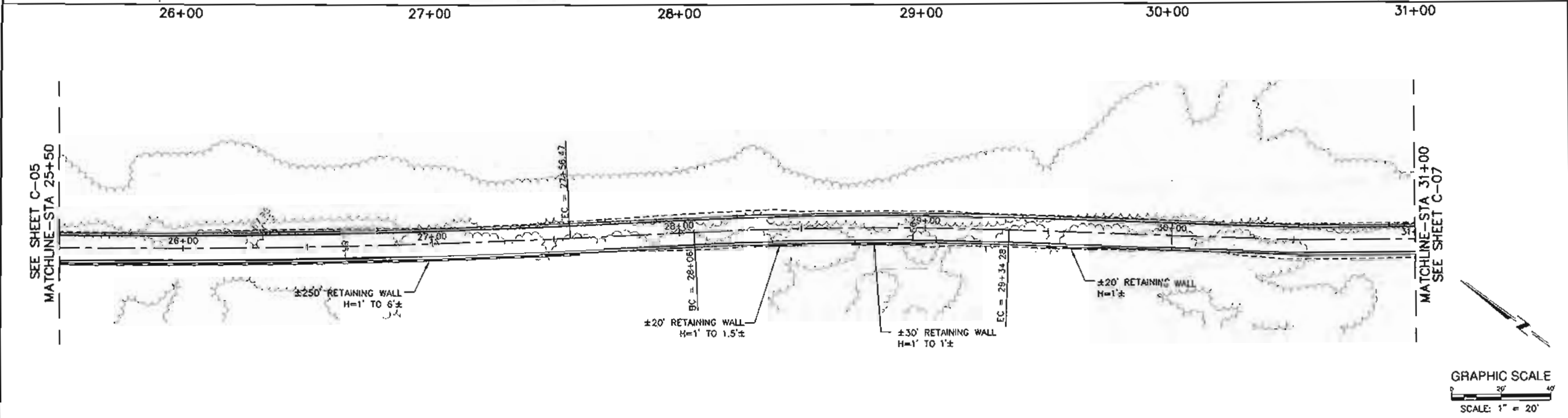
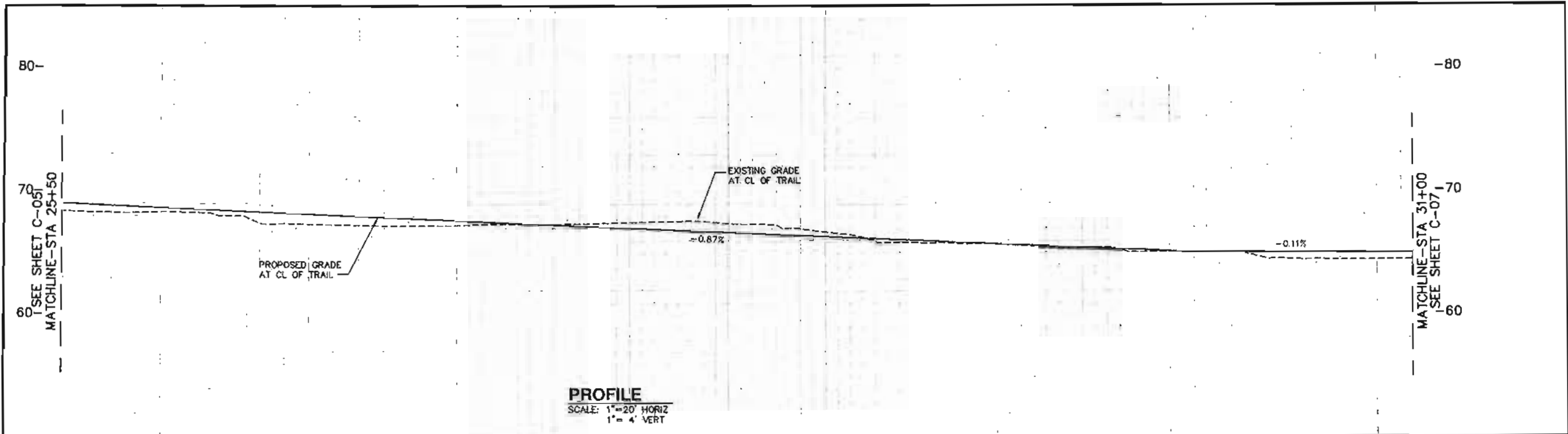
LINE IS 2" HIGHES AT FULL SIZE
IF NOT 2" - SCALE ACCORDINGLY

DUDEK
Dudek & Associates, Inc.
421 Chapala Street, Santa Barbara, CA
93101 805.963.0651 Fax 805.963.2074

PRELIMINARY DESIGN
NOVEMBER 3, 2008
NOT FOR CONSTRUCTION

CITY OF CARPINTERIA
PARKS & RECREATION
RINCON BEACH MULTI-USE TRAIL
PLAN AND PROFILE:
STA 20+00 TO STA 25+50
PRELIMINARY

SCALE: HORIZONTAL AS NOTED DATE: 11/03/08 SHEET 7 OF 12
VERTICAL AS NOTED



C-06

REV. NO.	DATE	DESCRIPTION	APPVD.	DATE
REVISIONS				

DESIGN: ICE & GB
DRAWN: GB & NSL
CHECKED: TDE

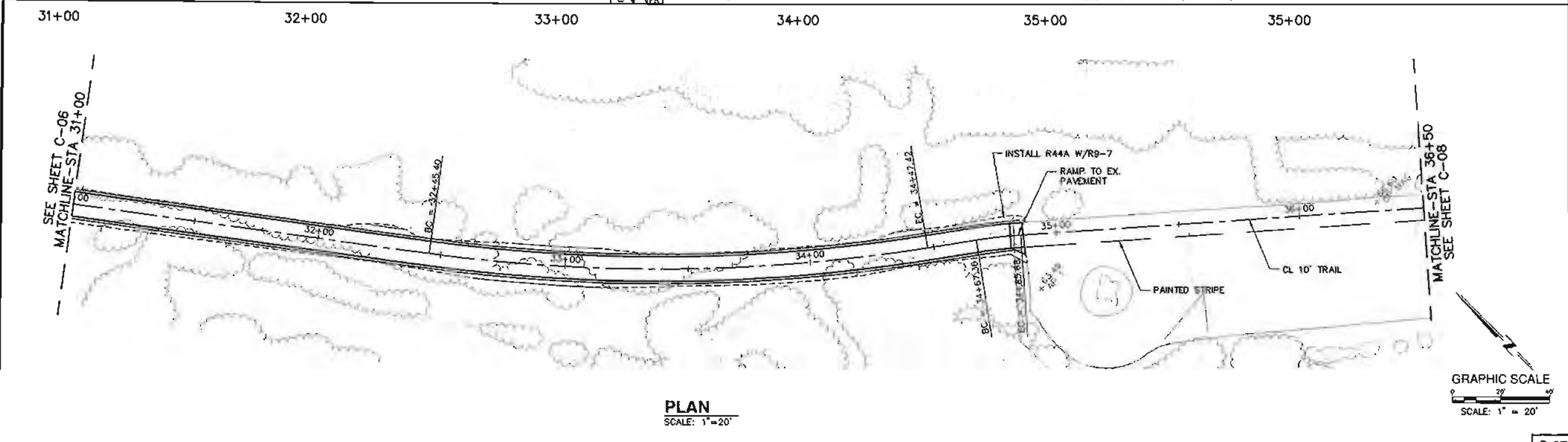
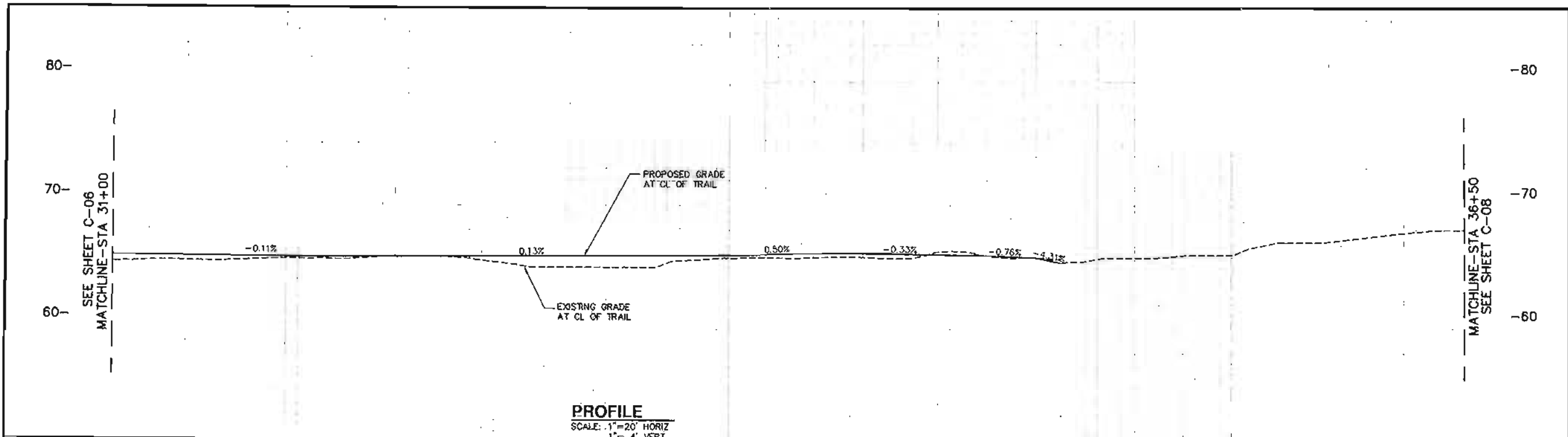
1" = 2" INCHES AT FULL SIZE IF NOT 2" - SCALE ACCORDINGLY

DUDEK
Dudek & Associates, Inc.
421 Chapala Street, San Francisco, CA 94101
805.963.0651 Fax 805.963.2074

PRELIMINARY DESIGN
NOVEMBER 3, 2008
NOT FOR CONSTRUCTION

CITY OF CARPINTERIA
PARKS & RECREATION
RINCON BEACH MULTI-USE TRAIL
PLAN AND PROFILE:
STA 25+50 TO STA 31+00
PRELIMINARY

SCALE: HORIZONTAL AS NOTED DATE: 11/03/08 SHEET 8 of 12
VERTICAL AS NOTED



C-07

REV. NO.	DATE	DESCRIPTION	APPVD.	DATE
REVISIONS				

DESIGN: TCE & GB
DRAWN: GB & NSL
CHECKED: TCE

1/8" = 2' HORIZ
1/8" = 2' VERT
SCALE ACCORDINGLY

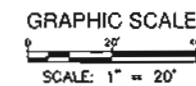
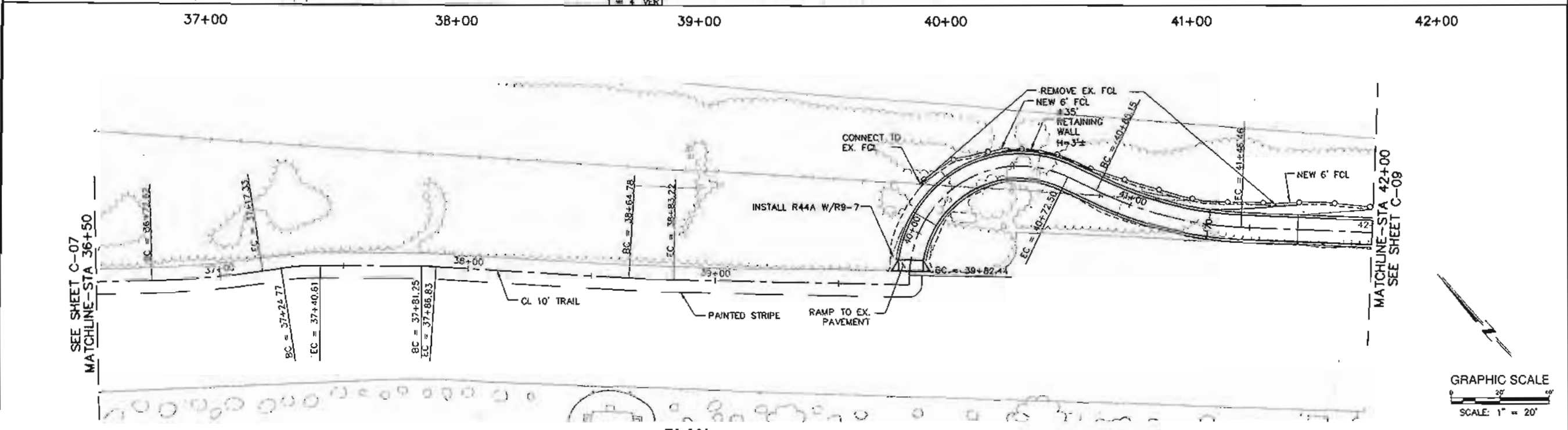
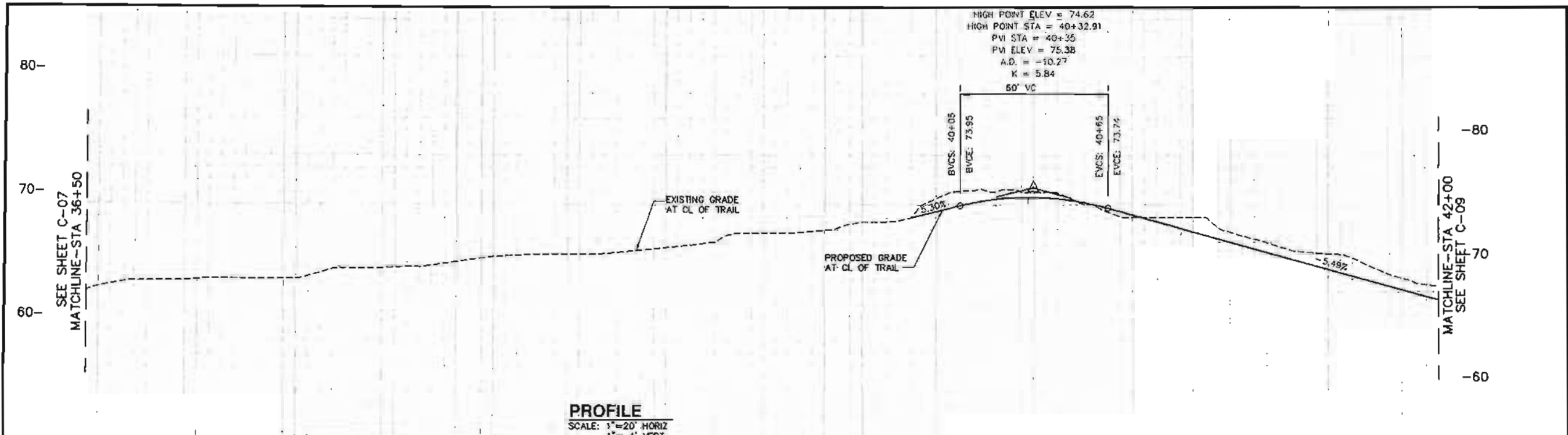


DUDEK
Dudek & Associates, Inc.
421 Chapel Street, Santa Barbara, CA
93101 805.963.0651 Fax: 805.963.2074

PRELIMINARY DESIGN
NOVEMBER 3, 2008
NOT FOR CONSTRUCTION

CITY OF CARPINTERIA
PARKS & RECREATION
RINCON BEACH MULTI-USE TRAIL
PLAN AND PROFILE:
STA 31+00 TO STA 36+50
PRELIMINARY

SCALE: HORIZONTAL AS NOTED DATE: 11/03/08 SHEET 9 of 12
VERTICAL AS NOTED



C-08

REV. NO.	DATE	DESCRIPTION	APPVD.	DATE
REVISIONS				

DESIGN: YCE & GB
DRAWN: GB & NSL
CHECKED: YCE

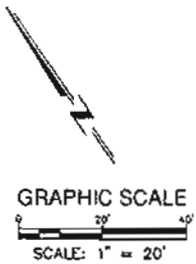
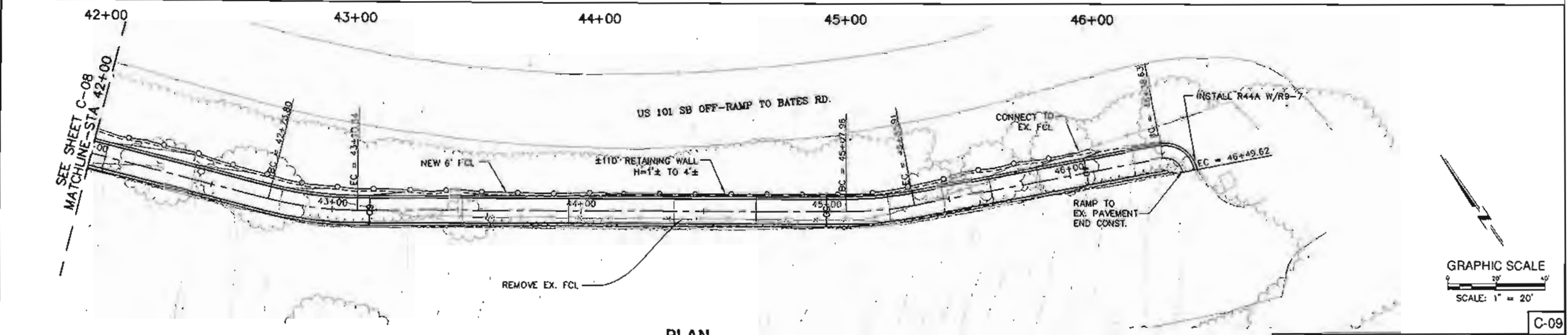
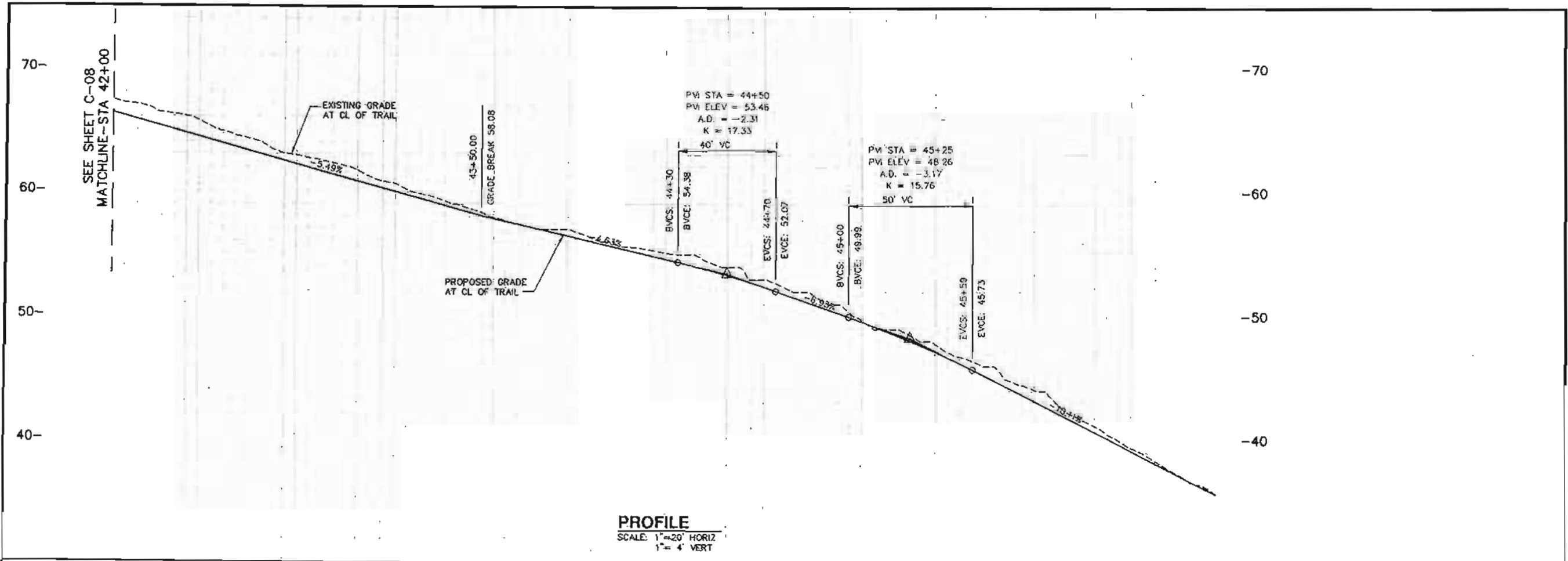
DUDEK
Dudek & Associates, Inc.
621 Chapala Street, Santa Barbara, CA
93101 805.967.0651 Fax 805.963.2074

DATE: 11/03/08
SCALE: 1" = 20' HORIZ
1" = 4' VERT

PRELIMINARY DESIGN
NOVEMBER 3, 2008
NOT FOR CONSTRUCTION

CITY OF CARPINTERIA
PARKS & RECREATION
RINCON BEACH MULTI-USE TRAIL
PLAN AND PROFILE:
STA 36+50 TO STA 42+00
PRELIMINARY

SCALE: HORIZONTAL: AS NOTED DATE: 11/03/08 SHEET 10 of 12
VERTICAL: AS NOTED



C-09

REV. NO.	DATE	DESCRIPTION	APPVD.	DATE
REVISIONS				

DESIGN: TCE & GB
DRAWN: GB & NSL
CHECKED: TCE

LINE IS 7/8" INCHES AT FULL SIZE
IF NOT 2" - SCALE ACCORDINGLY

DUDEK
Dudek & Associates, Inc.
421 Chapala Street, Santa Barbara, CA
93101 805.963.0651 Fax: 805.963.2074

PRELIMINARY DESIGN
NOVEMBER 3, 2008
NOT FOR CONSTRUCTION

CITY OF CARPINTERIA
PARKS & RECREATION
RINCON BEACH MULTI-USE TRAIL
PLAN AND PROFILE:
STA 42+00 TO STA 46+49.62
PRELIMINARY

SCALE: HORIZONTAL: AS NOTED DATE: 11/03/08 SHEET 11 of 12
VERTICAL: AS NOTED

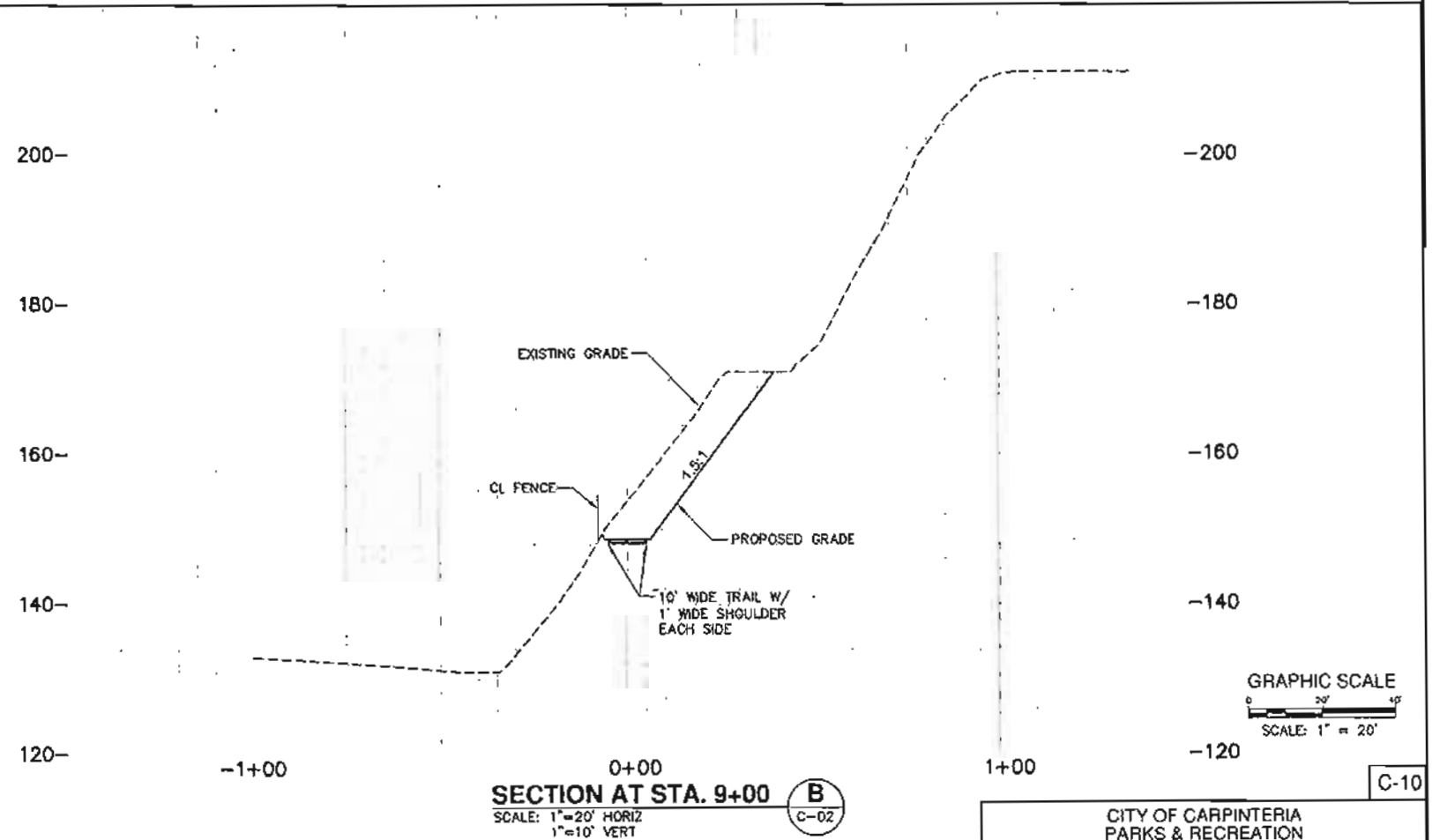
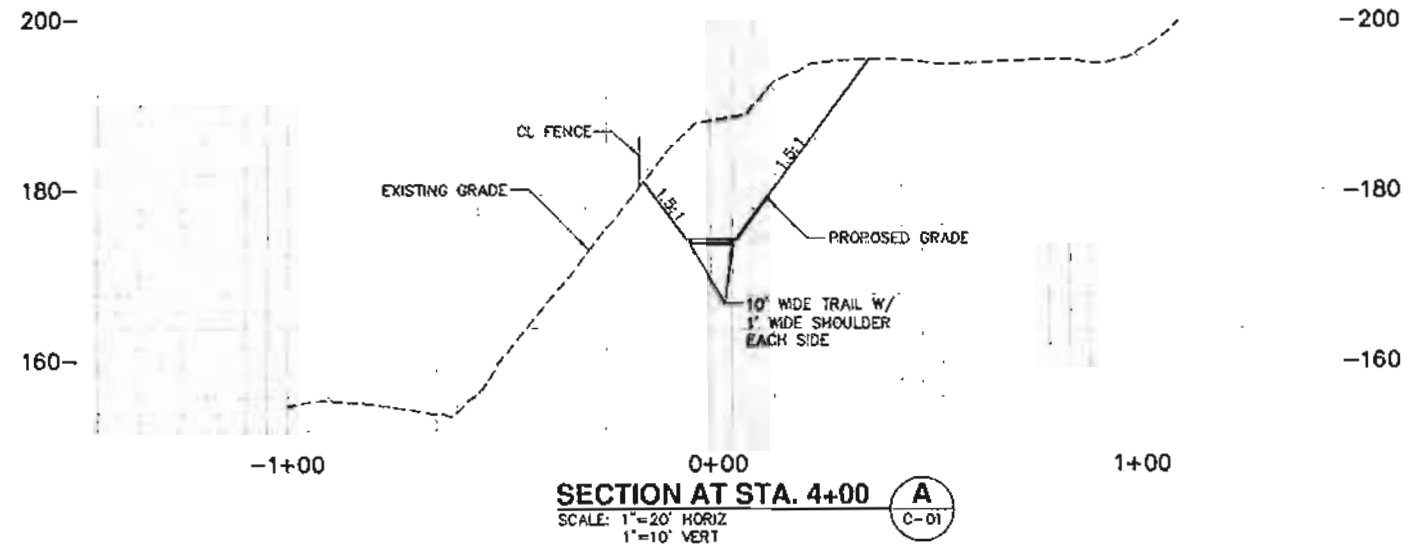
ENGINEERS ESTIMATED EARTHWORK VOLUMES

AREA	STA.	CUT (CY)	FILL (CY)
PARKING AREA	0+00 TO 1+00		
MATERIAL DISPLACED FOR 8" THICK IMPORTED AC PAVING & BASE			
		+140	0
PART 1	1+00 TO 13+50		
RAW VOLUME			
		12,108	10
MATERIAL DISPLACED FOR 6" THICK IMPORTED DG PAVING			
		+231	0
PART 2A	13+50 TO 15+75		
RAW VOLUME			
		144	11
MATERIAL DISPLACED FOR 6" THICK IMPORTED DG PAVING			
		+31	-10
PART 2B	17+00 TO 22+00		
RAW VOLUME			
		229	768
MATERIAL DISPLACED FOR 6" THICK IMPORTED DG PAVING			
		+46	-46
PART 3	22+00 TO 30+00		
RAW VOLUME			
		85	92
MATERIAL DISPLACED FOR 6" THICK IMPORTED DG PAVING			
		+74	-74
PART 4	30+00 TO 34+85		
RAW VOLUME			
		9	88
MATERIAL DISPLACED FOR 6" THICK IMPORTED DG PAVING			
		+22	-68
PART 5	39+82 TO 46+50		
RAW VOLUME			
		189	15
MATERIAL DISPLACED FOR 6" THICK IMPORTED DG PAVING			
		+124	0
TOTAL:		13,432	786
EXPORT:		12,646	

NOTES: "RAW VOLUME" IS THE CALCULATED DIFFERENCE BETWEEN THE EXISTING GROUND SURFACE AS REPRESENTED BY THE TOPOGRAPHIC MAP AND THE PROPOSED FINISHED GRADE SURFACE AS SHOWN ON THE DESIGN PLAN & PROFILE

NO ADJUSTMENT IS MADE FOR SHRINKAGE, SWELL, OR COMPACTION, OR FOR ANY OTHER FACTOR EXCEPT AS SHOWN IN THE TABULATION ABOVE

VOLUMES MAY CHANGE DUE TO CONDITIONS ENCOUNTERED IN THE FIELD AT THE TIME OF CONSTRUCTION



REV. NO.	DATE	DESCRIPTION	APP'D.	DATE
REVISIONS				

DESIGN: TCE & GB
DRAWN: GB & NSL
CHECKED: TCE

1/2" = 2' HORIZ
1" = 10' VERT

DUDEK
Dudel & Associates, Inc.
621 Chapel Street, Santa Barbara, CA
93101 805.963.0651 Fax 805.963.2074

PRELIMINARY DESIGN
NOVEMBER 3, 2008
NOT FOR CONSTRUCTION

CITY OF CARPINTERIA
PARKS & RECREATION
RINCON BEACH MULTI-USE TRAIL
CROSS SECTIONS STA 4+00 AND STA 9+00
& EARTHWORK VOLUME SUMMARY

PRELIMINARY

SCALE: HORIZONTAL AS NOTED DATE: 11/03/08 SHEET 12 OF 12
VERTICAL: AS NOTED

Carpinteria Rincon Trail

Attachment 5
Studies and Preliminary Plans
Electronic Copy



Picture Locations on Proposed Rincon Trail Route



Current Pictures of Proposed Rincon Trail Route

1. Looking East towards 101, where proposed bridge will be built.



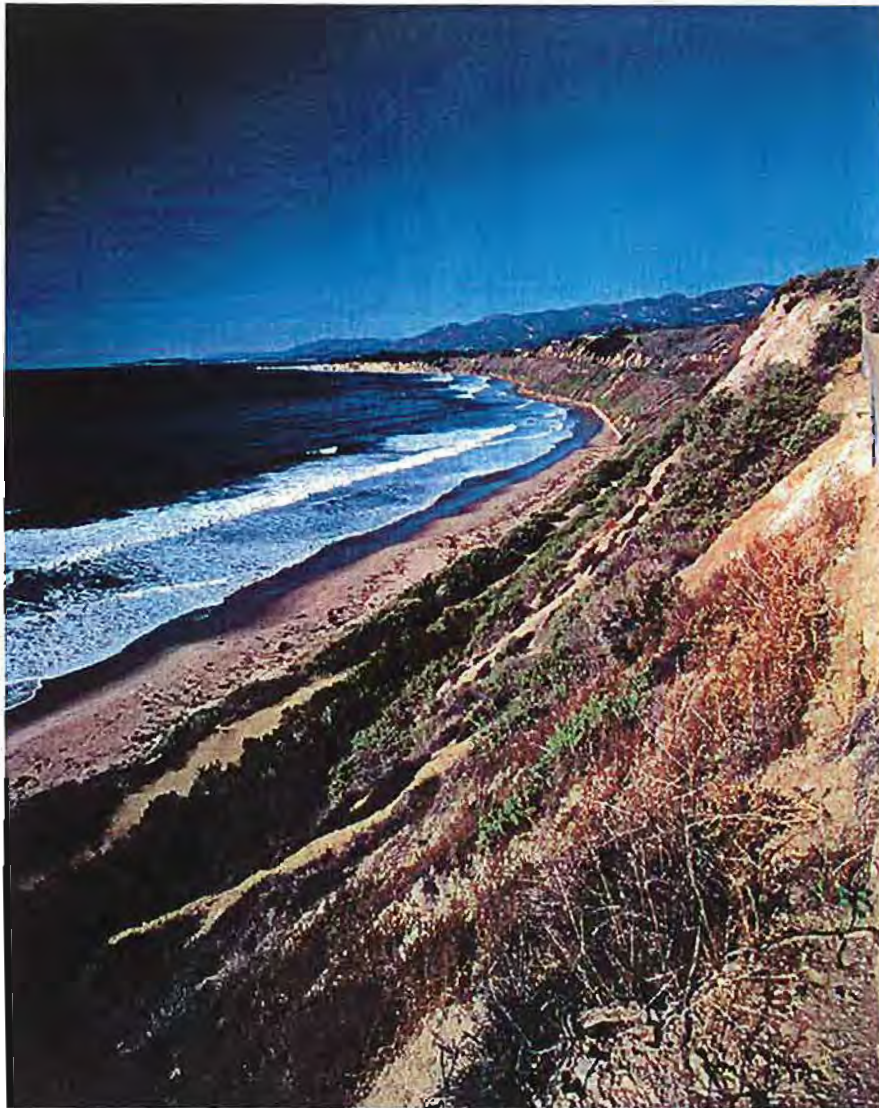
2. High end of trail right before proposed bridge over railroad tracks.



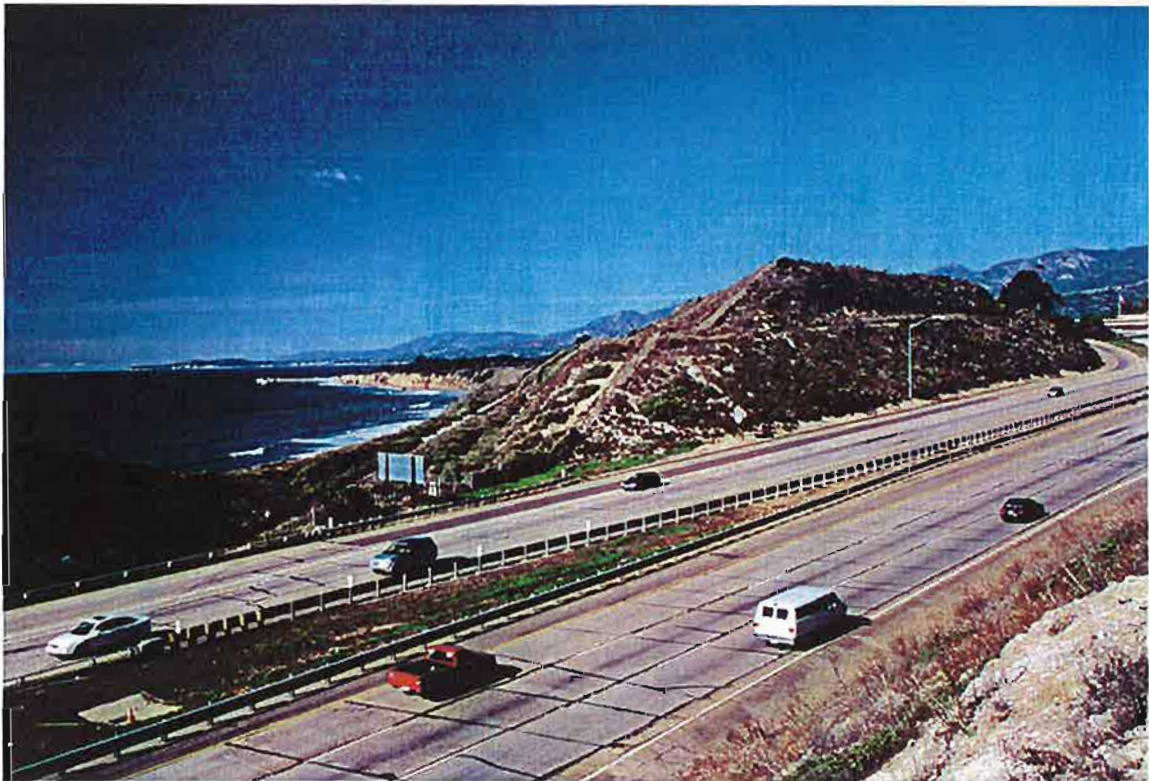
3. Look out from trail before proposed bridge. Views of Carpinteria Bluffs, Pacific Ocean, and Channel Islands.



4. South end of proposed bridge, south of railroad tracks. Looking back towards Carpinteria Bluffs Nature Preserve.



5. Taken from East Side of Highway 101. Looking west towards knoll will trail will turn down and head over bridge.



6. Where proposed trail will meet Rincon County Park.



7. On a high bluff top, looking back towards proposed trail, towards proposed pedestrian bridge. Trail would meander down along the left side of bluff top that is on the south side of railroad tracks.



8. Proposed trail route in bloom, looking towards Carpinteria Bluffs Nature Preserve.

